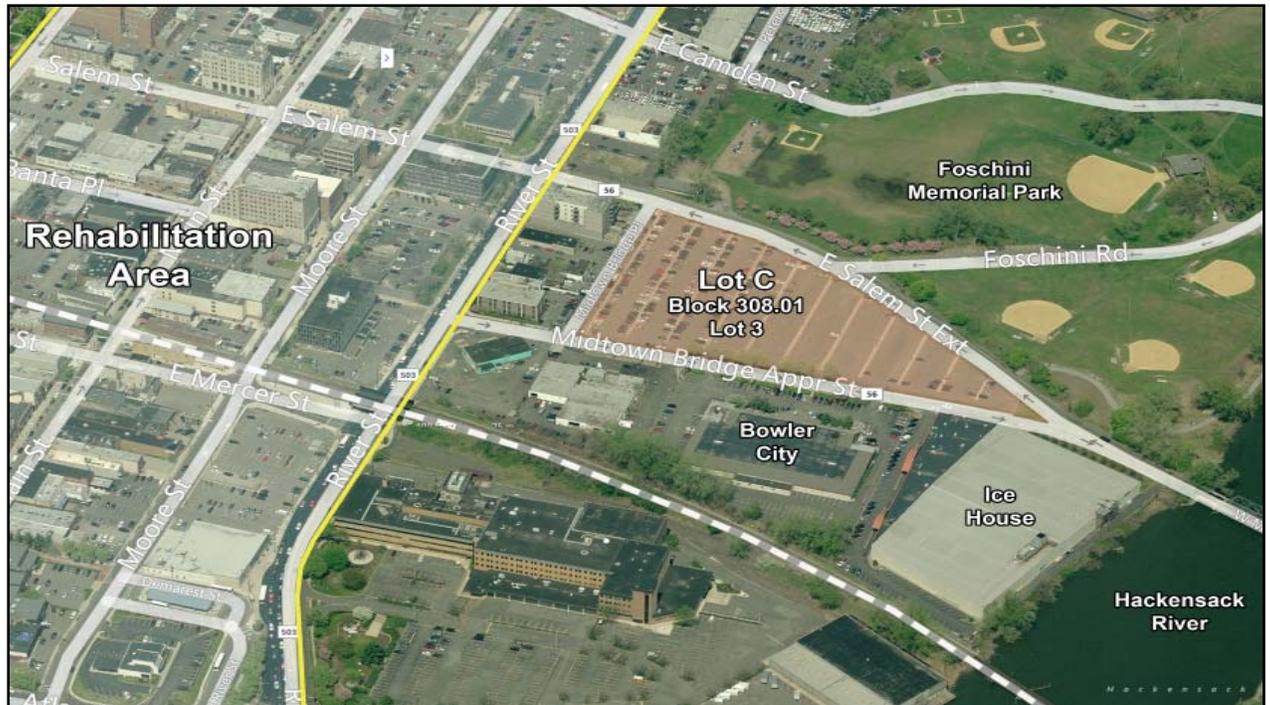


City of Hackensack

Lot 'C' Redevelopment Plan

Block 308.01: Lot 3



**CITY OF HACKENSACK,
BERGEN COUNTY, NEW JERSEY**

**January 2014
AMENDED March 2014**

Prepared by:
DMR Architects
777 Terrace Avenue
Hasbrouck Heights, NJ 07604

Francis Reiner, LLA - PP
License #: LI00616700

Date: January 2014

TABLE OF CONTENTS

Acknowledgements	ii
Background Information	iii
Introduction	1
Basis for the Plan	
Property Information	
Required Components of the Redevelopment Plan	
Definitions	
Redevelopment Plan Goals	
Redevelopment Plan Objectives	
Redevelopment Area Context	9
Surrounding Area Context	
Relationship to the 2001 Master Plan	
Relationship to the 2009 Reexamination Report	
Relationship to the Zoning Ordinance	
Use and Bulk Regulations	17
Permitted Land Uses	
Accessory Land Uses	
Density, Area, Yard & Height Regulations	
Parking Standards	
Conceptual Site Plans	
Development and Placemaking Design Standards	23
Architectural	
Streetscape	
Signage	
Relationship to Other Master Plans	33
State Development and Redevelopment Plan	
Bergen County Master Plan	
Neighboring Community Master Plans	

ACKNOWLEDGEMENTS

Mayor and Council

Mayor John Labrosse
Deputy Mayor Kathleen Canestrino
Councilman Leonardo Battaglia
Councilwoman Rose Greenman
Councilman David Sims

Planning Board

John Labrosse - Mayor
Kathy Canestrino - Deputy Mayor
Fernando Garip, Jr. - Chairman
Art Koster
John Chiusolo
Mike Allegretta
Nita Kundanmal
Ray Kaminsky
Stephen Pace
Dewey Aleem

Planning Board Attorney

Mr. Joseph L. Mecca Jr., Esq.

City Manager

Mr. Anthony Rottino

BACKGROUND INFORMATION

The City of Hackensack is in the midst of a renaissance. Over the past few years the City has taken tremendous steps in implementing and adopting new zoning through the creation of a Rehabilitation Plan which provides a clear vision to transform the downtown into a mixed use, pedestrian friendly environment.

The Plan which was adopted in 2012 promotes:

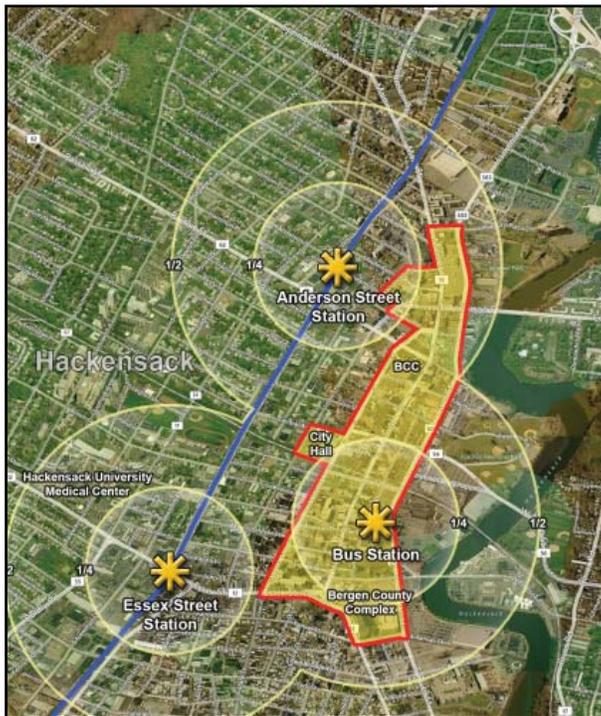
1. Smart growth principles by creating zoning which increases development flexibility, reduces parking ratios and promotes mixed-use, pedestrian friendly development in the downtown;
2. Connectivity to existing public infrastructure, including the two NJ Transit Rail Stations, the NJ Transit Regional Bus Station and Routes 4, 17, 46, Interstate 80 and the Garden State Parkway;
3. A mixture of uses with a variety of residential housing options to encourage walkability and active streetscapes;
4. Redevelopment and rehabilitation through architectural, neighborhood design standards that ensure high quality development;
5. The implementation of a two way street system; and
6. Strategies which include municipal tools and mechanisms to promote revitalization.

In only a short period of time since the adoption of the plan, the City has already begun to see the benefits of these strategies. For the first time in over thirty years, a significant residential development of approximate 220 units is under construction in the downtown. In addition, the City has seen a wave of developer interest throughout the rehabilitation area with several other mixed use projects that are either in the design or entitlement process.

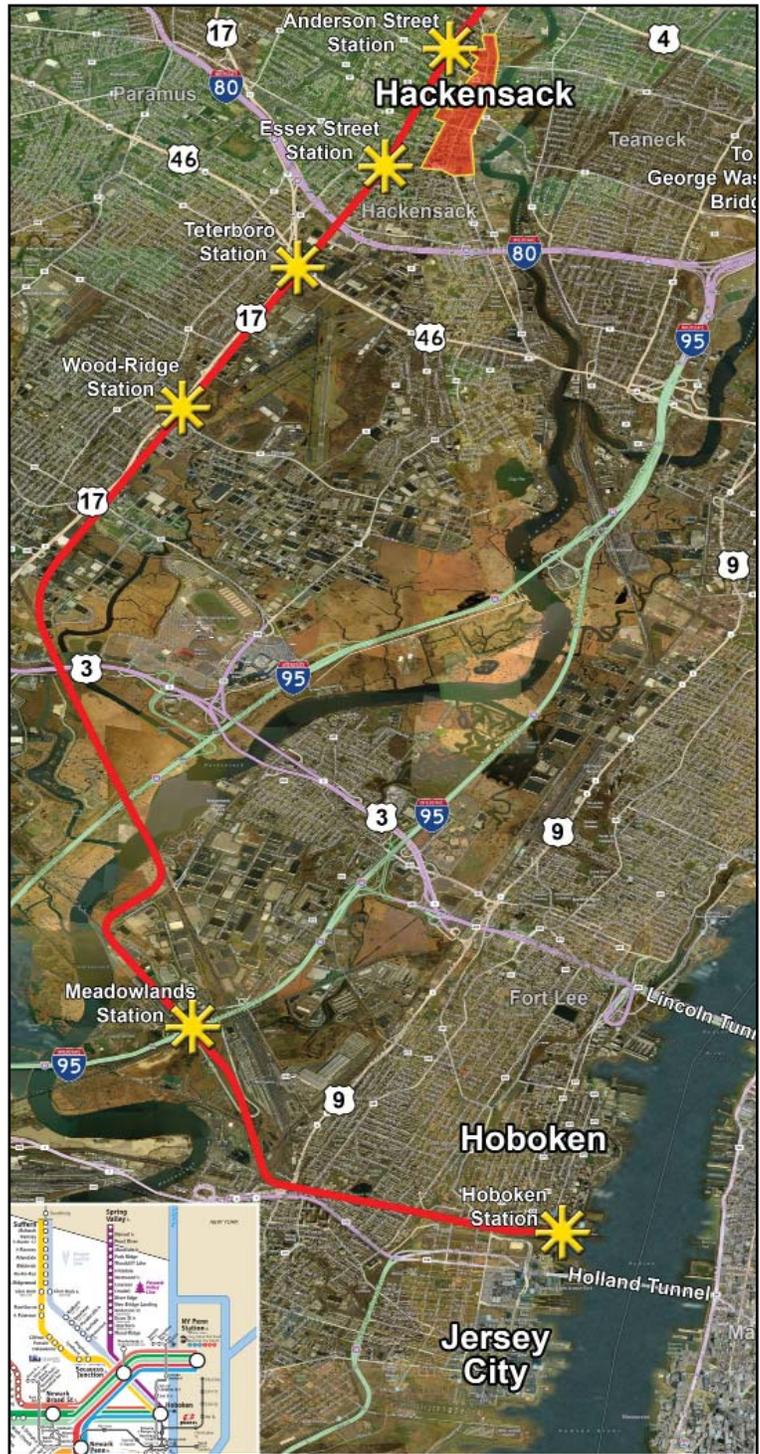
Hackensack represents a premier location in Bergen County that supports higher density mixed use residential development over the next 5 to 10 years. The City has two New Jersey Transit Stations which run directly to Hoboken as well as the regional New Jersey Bus Station that provides direct access throughout the county and to New York City. The downtown has access to Routes 4, 17, 46, Interstate 80 and the Garden State Parkway and is located eight miles from the George Washington Bridge, and 13 miles from the Lincoln Tunnel.

With over 43,000 residents and a day time population estimated at over 100,000 the City is home to the two largest employers in Bergen County including the Hackensack University Medical Center and the County Administration Offices. With the recent announcement of a partnership between Georgetown University and the Hackensack University Medical Center there will soon be four colleges/universities located in and adjacent to the downtown including, Fairleigh Dickinson University, Bergen Community College, Eastwick University and Georgetown University.

As a part of the on-going initiatives to promote revitalization within and adjacent to the downtown, the City of Hackensack designated Lot 'C', a city owned parking lot, as an area in need of redevelopment in June 2013. As a continuation of that process the City has put together this redevelopment plan in anticipation of reaching out to the development community to promote a predominantly residential mixed use project that would support the goals and objectives of the downtown rehabilitation plan.



Hackensack Rehabilitation Area
New Jersey Transit Locations with 1/4 and 1/2 mile radius



Hackensack Rehabilitation Area
New Jersey Transit Stops & Regional Proximity to Manhattan

INTRODUCTION

BASIS FOR THE PLAN

This redevelopment plan has been prepared for Block 308 – Lot 3 known as City of Hackensack Surface Parking Lot 'C'. The approximately 4.3 acre site is located between East Salem Street Extension, Midtown Bridge Approach, Midtown Place and is across from Foschini Park. It is one block off the Downtown Rehabilitation Area across River Street and represents a major gateway into the downtown from the adjacent communities to the east via the East Salem Street Bridge over the Hackensack River.

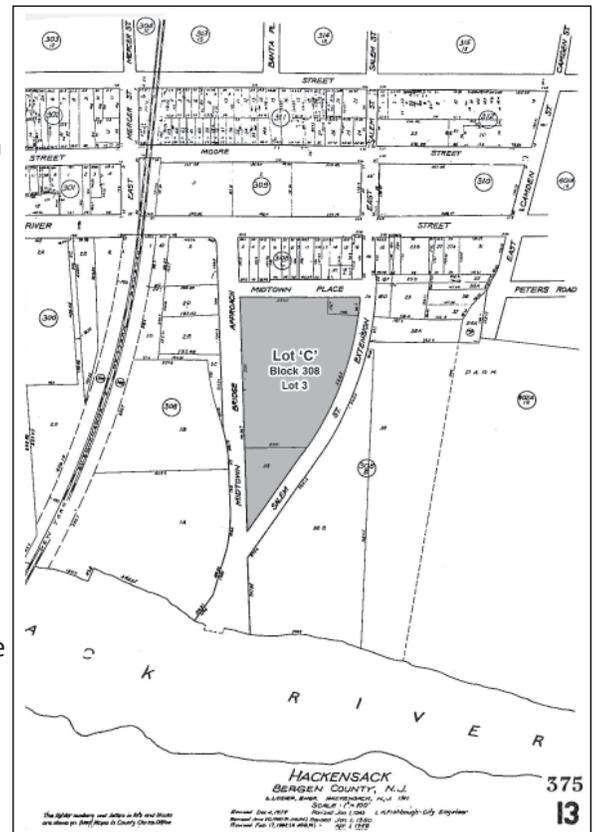
The City of Hackensack Mayor and Council in 2013 determined by resolution this property qualified as an "area in need of redevelopment" in accordance with the criteria specified in the New Jersey Local Redevelopment and Housing Law (LRHL) at N.J. S.A. 40A:12A-14. The LRHL allows a municipality to prepare a redevelopment plan which provides the development regulations and other standards to guide future development for this area.

PROPERTY INFORMATION:

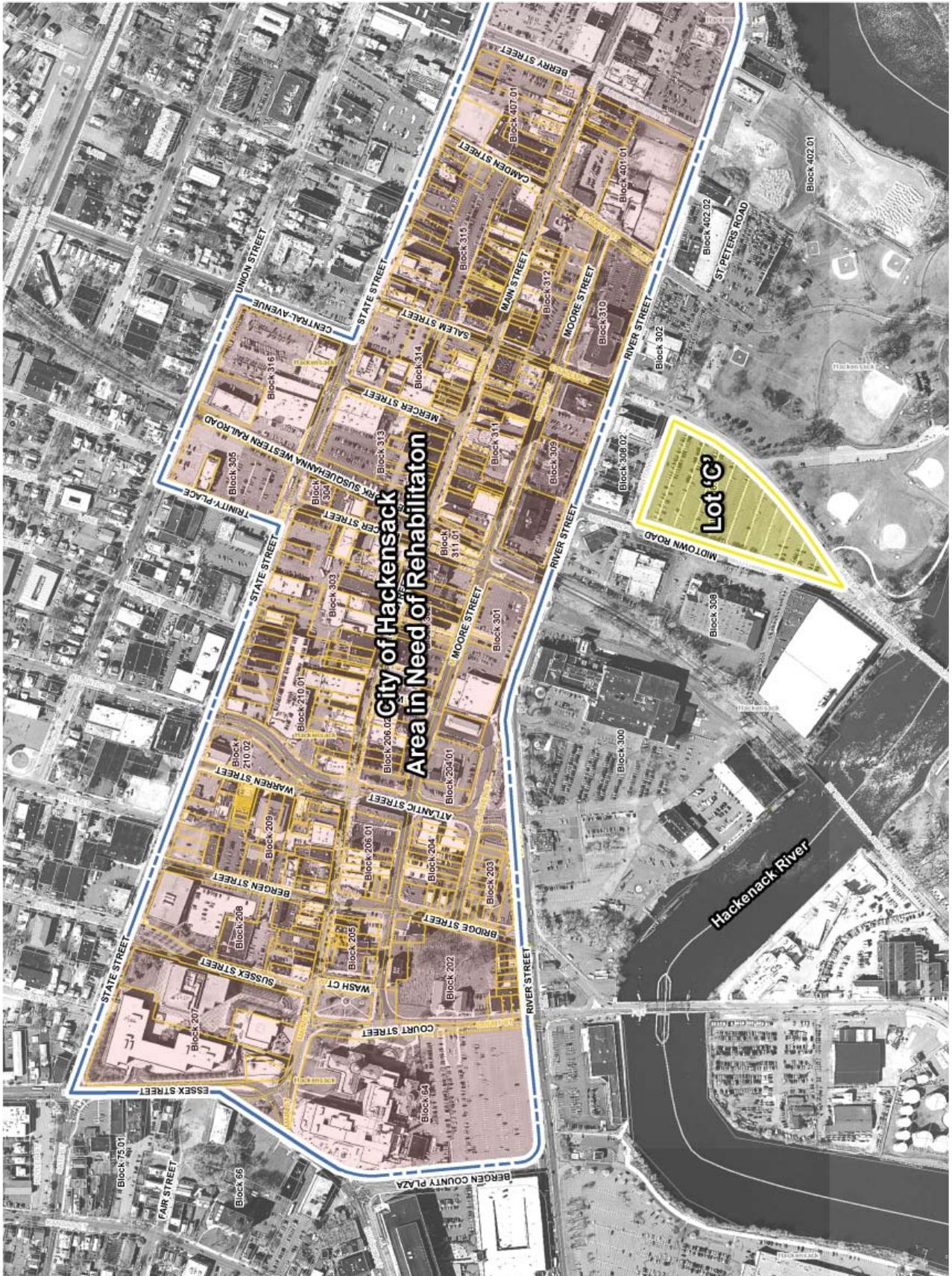
The parcel known as City of Hackensack parking Lot 'C' or parking Area C (Block 308.01 Lot 3) is owned by the City of Hackensack and is currently in the B-2 Zoning District. The current land use for the property is a surface parking lot comprised of approximately 544 spaces.

The City of Hackensack 2001 Master Plan Report indicates that the Area of Investigation is located in an area identified as being in a Flood Prone Area. In addition, based upon a review online FEMA maps posted after Superstorm Sandy, the Area of Investigation is located in the Advisory Base Flood Elevation, Advisory Zone A and Advisory Shaded Zone X. According to FEMA, Advisory Zone A is comprised of the area subject to storm surge flooding from the 1% annual chance coastal flood. These areas are not subject to high velocity wave action but are still considered high risk flooding areas.

According to FEMA, the Advisory Shaded Zone X map layer shows areas of moderate coastal flood risk outside the regulatory 1% annual chance flood but within the limits of the 0.2% annual chance flood level.



Planimetric Map
City of Hackensack - Block 308 Lot 3

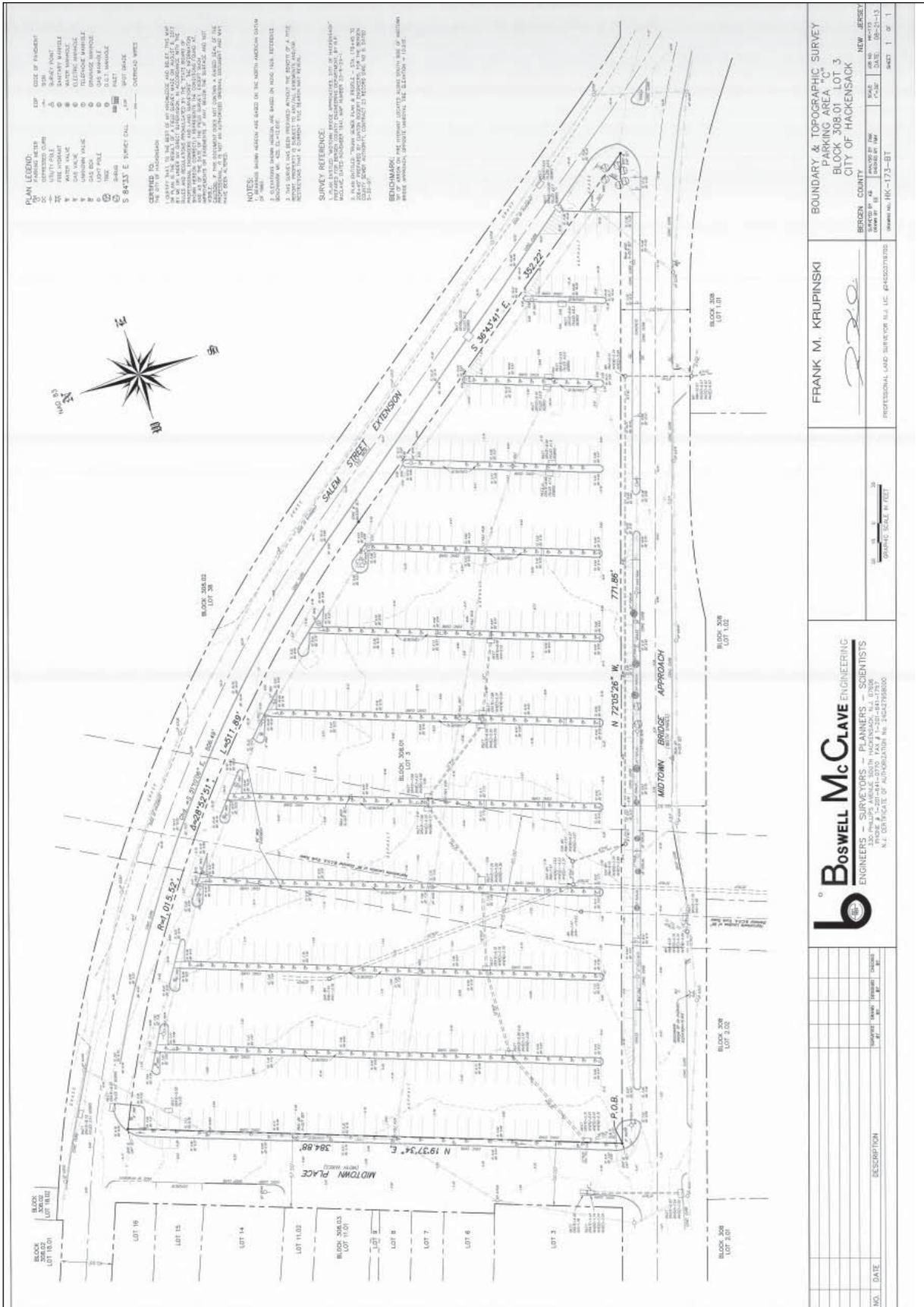


Hackensack Redevelopment Area
City of Hackensack - Location Map

REQUIRED COMPONENTS OF THE REDEVELOPMENT PLAN

N.J.S.A 40A:12A-7 requires that a redevelopment plan include an outline for the planning, development, redevelopment, or rehabilitation of the Redevelopment Area sufficient to indicate the following:

1. Its relationship to definite local objectives as to appropriate land use, density of population and improved traffic and public transportation, public utilities, recreational and community facilities and other public improvements.
2. Proposed land uses and building requirements in the project area.
3. Adequate provisions for the temporary and permanent relocation, as necessary for residents in the project area, including an estimate of the extent of which decent, safe, and sanitary dwelling units affordable to displace residents will be available to them in the existing local housing market.
4. An identification of any property with the redevelopment area which is proposed to be acquired in accordance with the redevelopment plan.
5. Any significant relationship of the redevelopment plan to:
 - The master plans of contiguous municipalities;
 - The master plan of the county in which the municipality is located;
 - The State Development and Redevelopment Plan adopted pursuant to the "State Planning Act" P.L. 1985,c.398 (C.52:18A-196 et al.).
6. An inventory (as of the date of the adoption of the resolution finding the area to be in need of redevelopment) of all housing units affordable to low and moderate income households, as defined pursuant to N.J.S.A. 52:27D-304, that are to be removed as a result of implementation of the redevelopment plan, whether as a result of subsidies or market conditions, listed by affordability level, number of bedrooms, and tenure.
7. A plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan .
8. The redevelopment plan may include the provision of affordable housing in accordance with the "Fair Housing Act," N.J.S.A. 52:27D-301 et seq. and the housing element of the municipal master plan.
9. The redevelopment plan shall describe its relationship to pertinent municipal development regulations as defined in the "Municipal Land Use Law," P.L.1975, c. 291 (C.40:55D-1 et seq.).
10. The redevelopment plan must state whether it shall supersede applicable provisions of the development regulations of the municipality or constitute an overlay zoning district within the redevelopment area.
11. All provisions of the redevelopment plan shall be either substantially consistent with the municipal master plan or designed to effectuate the master plan; but the municipal governing body may adopt a redevelopment plan which is inconsistent with or not designed to effectuate the master plan by affirmative vote of a majority of its full authorized membership with the reasons for so acting set forth in the redevelopment plan.



Boswell Engineering Survey
City of Hackensack - Block 308 Lot 3

DEFINITIONS:

It is the intention of this Redevelopment Plan for the Main Street Rehabilitation Area to supersede existing zoning (except as noted herein) as provided under the Local Redevelopment and Housing Law, N.J.S.A 40A:12A-1, et seq. However, this plan adopts the definitions of the City's Land Use Ordinance Section 175-2.2 by reference.

The definitions of the City's Land Use Ordinance shall apply to this plan, unless this plan provides a superseding definition. The definitions defined in Section 175-2.2 apply to the redevelopment area with the exception of the following terms:

FACADE: Any exterior building face or wall.

STREET FURNITURE: The elements of streetscape, including but not limited to benches, bollards, news-racks, trash receptacles, tree grates, hardscape, seat-walls, street lights, and street trees.

BUILDING PROPORTION: The width to height relationship of one building to another.

PEDESTRIAN SCALE: The relationship of a particular building, in terms of mass and scale to a pedestrian.

VISUAL COMPATIBILITY: The design of buildings which is consistent in scale and character with adjacent buildings in the area from a pedestrian perspective in which the design is consistent with the architectural and neighborhood design standards set forth in this document.

BUILDING BASE: The building base refers to the first three levels of a particular building:

BUILDING HEIGHT: The vertical distance of a building measured from the average of the top of curb of the two corners of the proposed building. For projects located at a corner lot, the height will be measured from the top of curb of the two corners located along the primary building facade.

FENCE: An artificial barrier constructed of durable materials including brick, stone, other masonry material, wood posts and planks, metal or other materials designed for fencing with a maximum height of four feet. (A chain link fence of any type does not satisfy the definition of a fence)

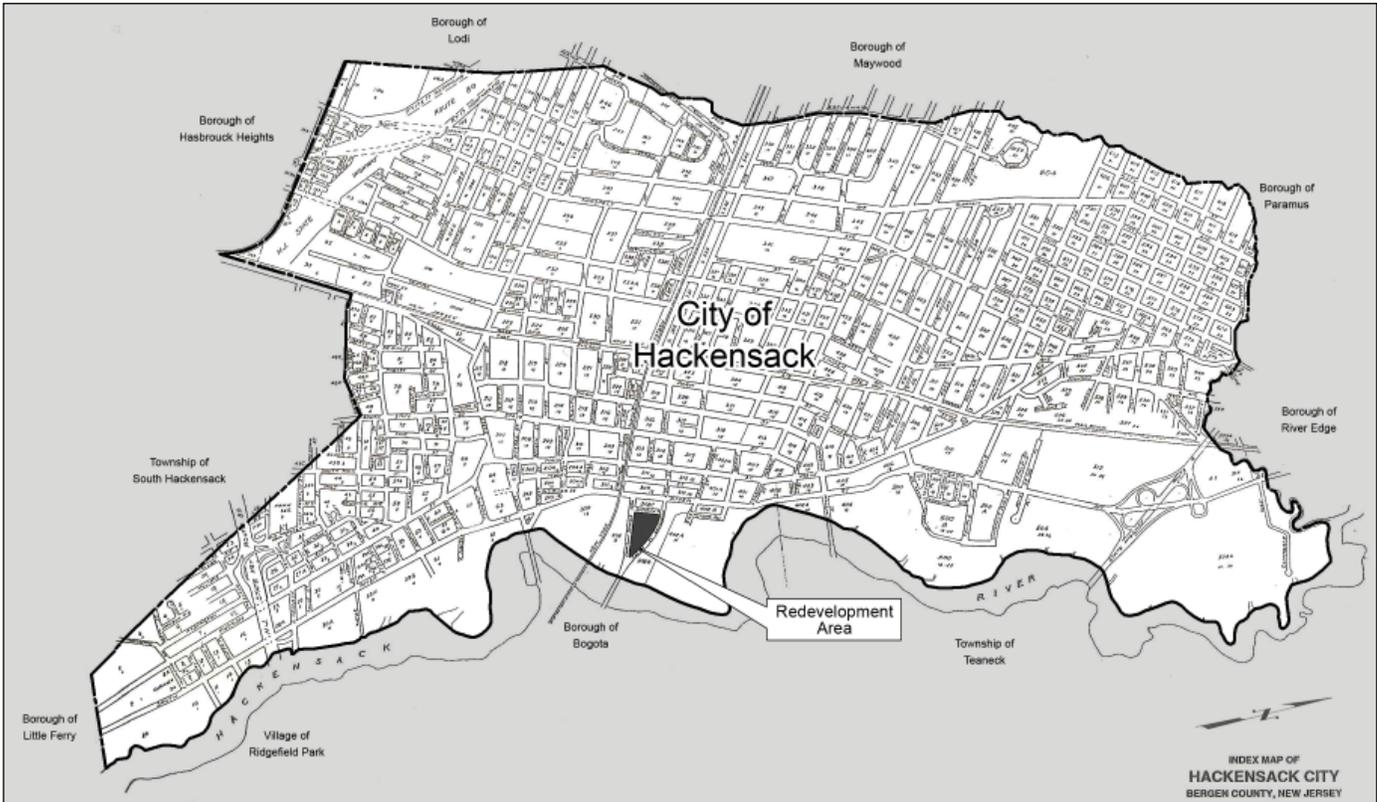
COMPLETE STREET: Roadways designed and operated to enable safe, attractive, and comfortable access and travel for all users, including pedestrians, bicyclists, motorists and public transportation users of all ages and abilities.

POP-OUT / PROTRUSION: A portion of the primary building facade that projects into the building setback.

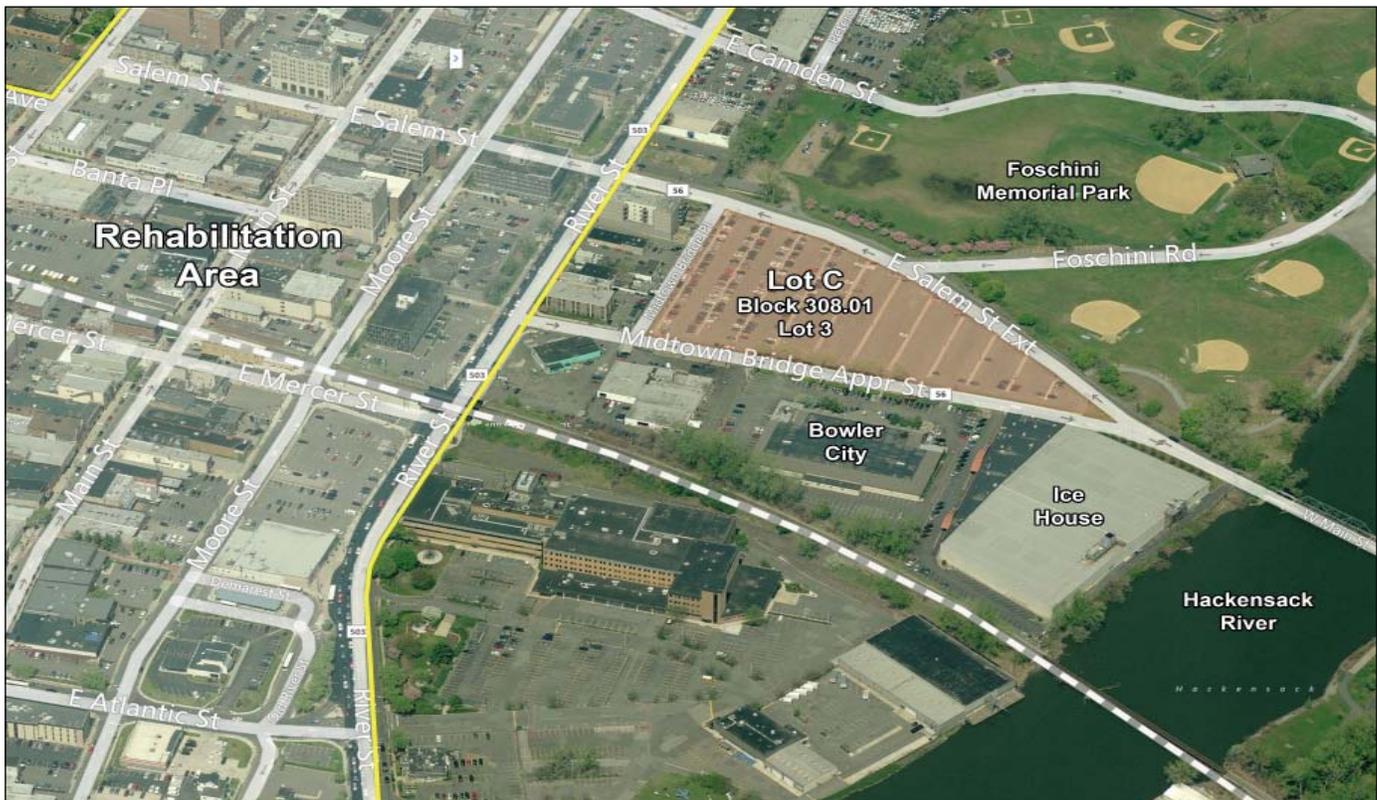
REDEVELOPER: Any person, firm, corporation or public entity that shall voluntarily seek and be designated as a Redeveloper by the City Council or Redevelopment Entity and shall enter into a Redevelopment Agreement as set forth in Section 5 of this Rehabilitation Plan, all in accordance with the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-1 et seq.

RESTAURANT: A building or structure designed, used or intended for use in which food and beverages are sold and consumed.

BUILDING SETBACK LINE: The distance measured from the back of existing or proposed curb (whichever is greater) to the primary building facade.



Context Map
City of Hackensack - Block 308 Lot 3



Bird's Eye Perspective
City of Hackensack - Block 308 Lot 3

REDEVELOPMENT PLAN GOALS:

This property is identified as a potential redevelopment area in both the 2001 Master Plan and the 2009 Re-examination Report and as a part of the Downtown Rehabilitation Planning process the city and its residents developed goals and objectives for the revitalization of the downtown into a vibrant mixed use pedestrian friendly environment. The following is a list of the goals which relate to the redevelopment area:

1. To promote and strengthen existing businesses with the creation of a livable, real and clearly defined downtown district, made up of a series of interconnected, newly formed, mixed-use neighborhoods which are connected to their immediate context;
2. To promote the development of a place-based environment where people live and work connected by great streets and activated by appropriate street retail, food and entertainment uses;
3. To promote a balance between pedestrian and vehicular importance through the design of a hierarchy of streets, roads, gateways and boundaries which promotes activity and vitality;
4. To promote a series of memorable public places and spaces connected by great sidewalks and streetscape elements, enhanced by public art, safe lighting and on-street parallel parking, that promote outdoor dining, walking, people watching and gathering, activated by a clearly organized program of street retail and restaurants;
5. To promote improved pedestrian and vehicular connectivity into and through the downtown and to adjacent existing residential and commercial neighborhoods as well as to transportation and transit options;
6. To promote an overall approach to parking based on a shared vertical strategy reflective of a mixed use environment which emphasizes quality, accessibility, location, size, scale, configuration, management and aesthetic character over quantity;
7. To promote and take advantage of the physical relationship of the downtown to the County Complex, Bergen Community College, Fairleigh Dickinson University and Hackensack University Medical Center;
8. To promote the development of a compact urban environment with improved connectivity to the existing rail and bus transit in order to encourage walking and minimize vehicular dependency;
9. To promote and improve the efficiency and capacity of the existing street network to better accommodate vehicles, pedestrians and bicycles in the context of a complete street;
10. To promote a high density, mixed use development with a diverse residential population of varied ages, races and socio-economic backgrounds;
11. To promote the principles of sustainable design for individual buildings and for the district as a whole.

REDEVELOPMENT PLAN OBJECTIVES:

The City is looking to promote a mixed use predominantly residential development that would support the continuing revitalization efforts in the downtown as a part of the 2001 Master Plan, 2009 Re-examination Report and the 2012 Downtown Rehabilitation Plan.

The objectives for this Redevelopment Plan are to:

1. Promote increased quality of life for all residents in the City of Hackensack;
2. Promote the development of a mid to high density predominantly residential mixed use project;
3. Support existing businesses and property owners while creating new construction and permanent jobs in the City;
4. Generate new tax ratables and revenue from the sale of the property to support additional revitalization activities;
5. Implementation architectural, neighborhood and sustainable design standards that promote high quality development.
6. Provide a public parking component in the redevelopment area to support surrounding businesses.

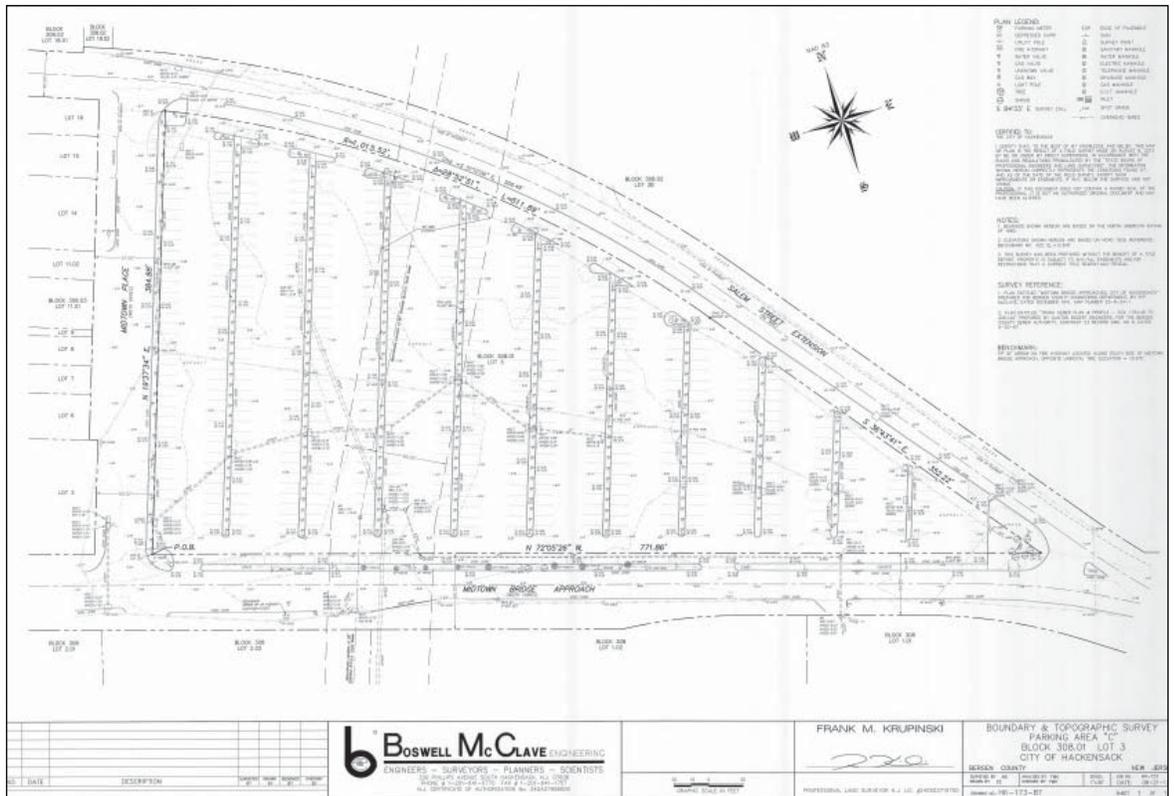
The redevelopment plan is intended to capitalize on its prime location and proximity to the Downtown Rehabilitation Plan, the New Jersey Regional Bus Station, the two New Jersey Transit Stations (Essex and Anderson Street), as well as to the major transportation including Route 4, 46, 17, Interstate 80 and the Garden State Parkway.

REDEVELOPMENT AREA CONTEXT

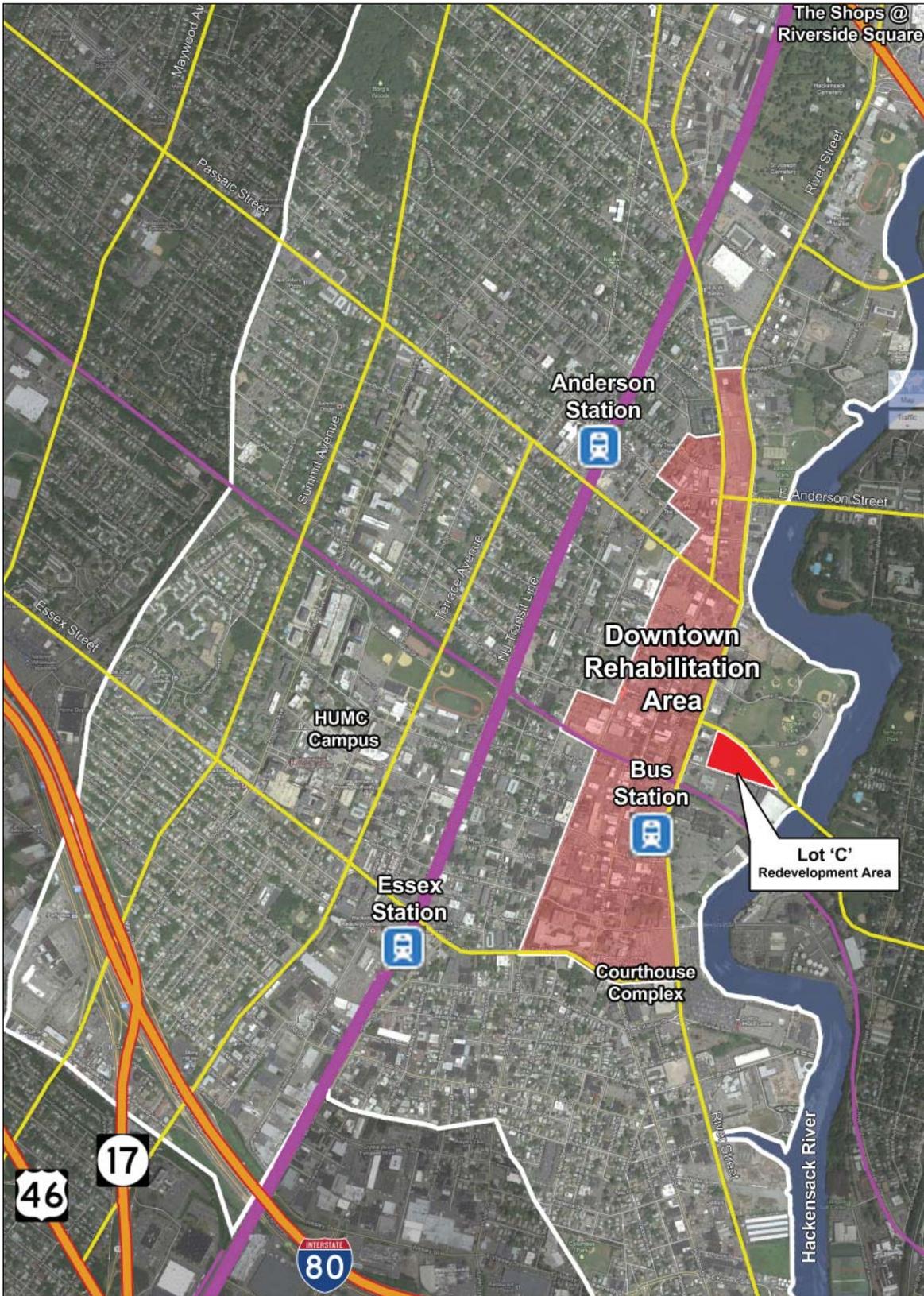
SURROUNDING AREA CONTEXT:

This redevelopment area consists of Block 308 – Lot 3 (Typically known as City of Hackensack - Lot 'C') which is approximately 4.3 acres and located between East Salem Street Extension to the north, Midtown Bridge Approach to the South and Midtown Place to the West. The property is located across from Foschini Park, one block off the Downtown Rehabilitation Area across River Street and is a major gateway into the downtown from Bogota to the east via the East Salem Street Bridge over the Hackensack River.

The redevelopment area is located in the eastern portion of the municipality one block to the east of the Downtown Rehabilitation Plan in what is identified in the 2001 City of Hackensack Master Plan Report as the Banking, Educational and Cultural Sphere of Influence. It was subsequently identified as a Redevelopment Target Area in the 2009 Master Plan Re-examination Report which specifically recommended residential development for this area.



Boswell Engineering Survey
City of Hackensack - Block 308 Lot 3



Context Map
City of Hackensack - Block 308 Lot 3

RELATIONSHIP TO THE 2001 MASTER PLAN

2009 MASTER PLAN REEXAMINATION REPORT:

In the 2009 Master Plan Re-examination Report the following information is relevant to the redevelopment area:

- The re-examination report noted that the 2001 Land Use Plan suggested three areas to be considered for redevelopment, including “an area to the east of River Street and north of New York Susquehanna Railroad and south of Anderson Street, area on both sides of Essex Street between the Passaic Valley (north/south) Line and State Street and lands between River Street and Moore Street and to the south of the New York Susquehanna Railroad and north of Bergen Street.
- 2009 re-examination report includes Map 1 “Redevelopment Target Areas 2001 & 2006” which identifies three areas which includes the redevelopment area to be considered for redevelopment.
- The re-examination report states that in evaluating the market opportunities, ERA suggested, “multi-family housing, primarily rental apartments in the downtown would create critical mass to spur development.
- The report also states that the downtown area lies mostly within the B-1 and B-2 Districts which allows multi-family residential and mixed-use development.

2001 MASTER PLAN REPORT:

The City of Hackensack 2001 Master Plan Report and 2009 Master Plan Reexamination Report provides the following information that is relevant to the Area of Investigation:

- Maintain and Enhance the Quality of Established Neighborhoods in Hackensack;
- Assist in the Rehabilitation of Areas in Need of Improvement and Upgrading including utilization of State and Federal Assistance programs, where applicable, as well as private efforts;
- Provide Housing Opportunities and A Variety of Housing for Various Income Levels of the population including low and moderate income housing, middle income housing and senior citizen housing;
- Improve the Quantity, Quality and Availability of Parks and Open Space including active and passive recreational facilities, neighborhood parks and environmentally sensitive areas and promote the establishment of a publicly accessible linear greenway (riverside) park along the Hackensack River.
- Preserve the Environment including the preservation of wetland areas, stream and wetland corridors; the quality and purity of rivers and streams, protection of major ridgelines and the significant treed areas and areas designated as natural areas.
- Promote Historic Conservation, Preservation and Adaptive Reuse Where Appropriate.

- Promote Adequate Community Services for All Portions of Hackensack.
- Improve and Upgrade Traffic Circulation and Safety Through-out the City including the provision of additional passenger rail service.
- Promote and Expand Economic Opportunities in Hackensack in Order to Maintain Job Opportunities and the Economic Well-Being of Hackensack's Citizens.
- Promote and Upgrade the Downtown Area of Hackensack Including the Four Spheres of Influence.
- Promote Upgrading Surface Water Quality of all rivers and streams entering Hackensack and preserve water quality within the City.

The 2001 Master Plan also incorporates the general purposes of the Municipal Land Use Law as part of Hackensack' Master Plan which includes the following:

- To encourage municipal action to guide the appropriate use or development of all lands in this State, in a manner which will promote the public health, safety morals, and general welfare;
- To secure safety from fire, flood, panic and other natural and manmade disasters;
- To provide adequate light, air and open space;
- To ensure that the development of individual municipalities does not conflict with the development and general welfare of neighboring municipalities, the county and the State as a whole;
- To promote the establishment of appropriate population densities and concentrations that will contribute to the well-being of persons, neighborhoods, communities and regions and preservation of the environment;
- To encourage the appropriate and efficient expenditure of public funds by the coordination of public development with land use policies;
- To provide sufficient space in appropriate locations for a variety of agricultural, residential, recreational, commercial and industrial uses and open space, both public and private, according to their respective environmental requirements in order to meet the needs of all New Jersey citizens;
- To encourage the location and design of transportation routes which will promote the free flow of traffic while discouraging location of such facilities and routes which result in congestion or blight;
- To promote a desirable visual environment through creative development techniques and good civic design and arrangements;
- To promote the conservation of historic sites and districts, open space, energy resources and valuable natural resources in the State and to prevent urban sprawl and

degradation of the environment through improper use of land;

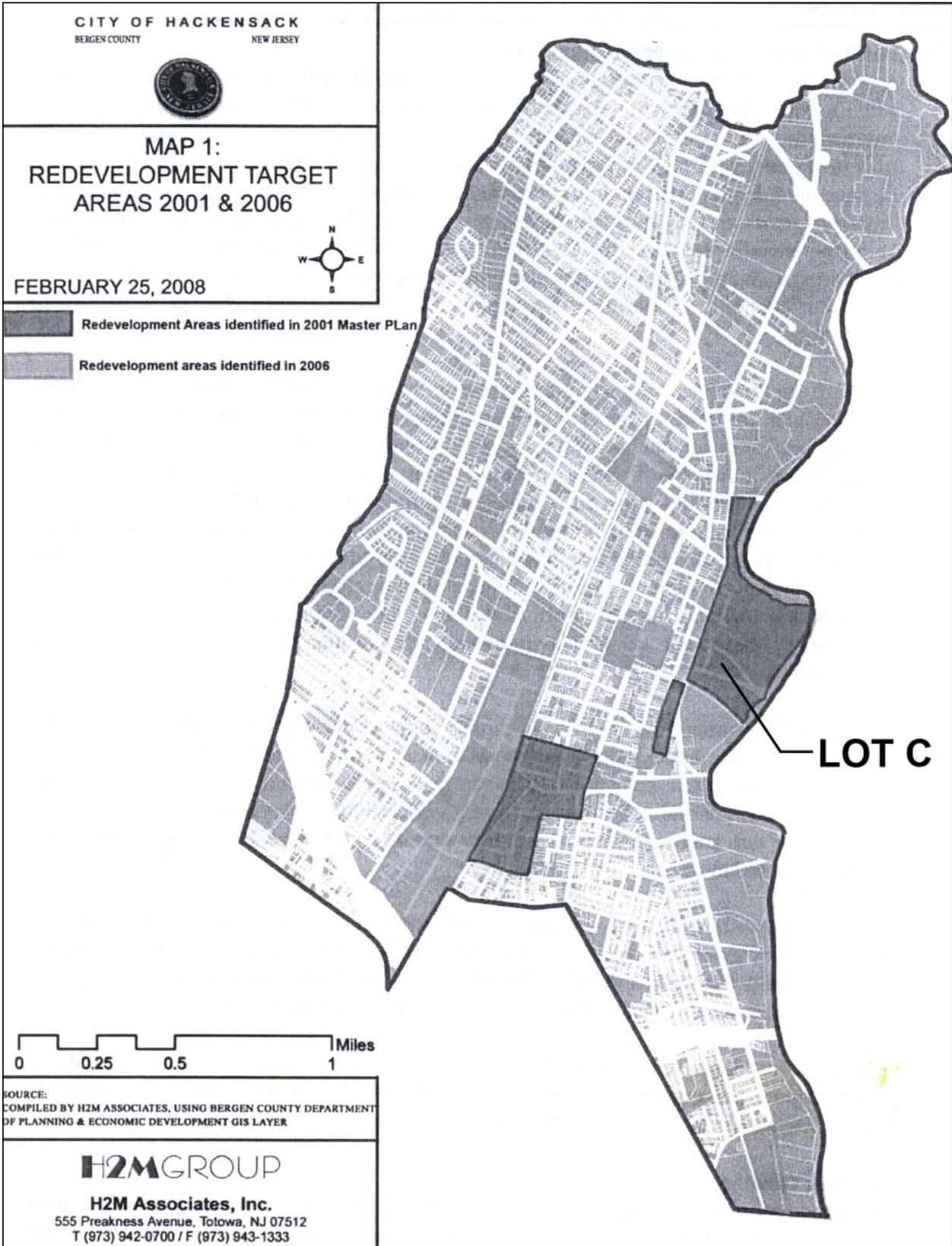
- To encourage planned unit developments which incorporate the best features of design and relates the type, design and layout of residential, commercial, industrial and recreational development to the particular site;
- To encourage senior citizen community housing construction;
- To encourage coordination of the various public and private procedures and activities shaping land development with a view of lessening the cost of such development and to the more efficient use of land;
- To promote utilization of renewable energy resources; and
- To promote the maximum practicable recovery and recycling of recyclable materials from municipal solid waste through the use of planning practices designed to incorporate the State Recycling Plan goals and to complement municipal recycling programs.

The 2001 City of Hackensack Master Plan Report includes a "Downtown Area Evaluation" which references an analysis from Economic Research Associates (ERA) report entitled "Downtown Market Analysis and Strategy". The following relevant information includes:

1. The report identifies "Four Spheres of Influence" in the downtown area one of which includes the Banking, Educational and Cultural Sphere where the redevelopment area is located. The document includes the following:
 - The key to residential development in the City relates to redevelopment, both public and private, to update and create new housing opportunities in areas appropriate for such activity.
 - The entire easterly frontage along River Street extending from the railroad to the Passaic Street intersection should be considered for redevelopment purposes, including public parking facilities, apartment uses, and/or commercial development.
 - A second area that is likely an eligible area for redevelopment lies along the east side of River Street, north of the Susquehanna Railroad extending to Anderson Street and to the Hackensack River. This area, predominantly developed with automobile sales establishments and related sales and services, could serve as a significant area for mid-rise to high-rise apartment uses. The relative proximity to the Hackensack River and a proposed riverwalk greenway system, the proximity to retail sales and services could work in a symbiotic manner. The area could also provide for additional retail and service uses to augment downtown uses along Main Street.
2. The Land Use Plan recommendations in the report include the following as it relates to the Banking, Educational and Cultural Sphere of Influence:
 - River Street, a major thoroughfare in Hackensack, is primarily occupied by automotive related uses extending the full width of this Sphere of Influence. Due to the nature of the

automotive sales, land uses tend to be underutilized in terms of automotive storage of new and used vehicles. Other functions such as automobile related services, such as tires, collision work, gas stations and the like do not relate well to the compact pedestrian related type uses in the downtown area. This area also contains a greater lot depth extending to the Hackensack River, and could provide an area of greater retail and residential use, a condition that is presently lacking along Main Street.

- Parking in Lot C located between the Salem Street Extension and the Midtown Bridge Approach serves Foschini Park, the two commercial-recreation uses in the area and some of the nearby development, but is not particularly convenient for development along Main Street and Moore Street.
- Additional parking, particularly garage parking should be considered for this area.
- The Plan for the area suggested that investigation be given to several sites as possible public/private redevelopment areas. These include the entire easterly side of River Street extending from the railroad to the extension of Passaic Street. This area could serve as a major redevelopment program due to its relative size, access to River Street, a major thoroughfare in Hackensack, and its proximity to the present Main Street and the potential train station for the Susquehanna Railroad. Nearby uses could include apartment uses overlooking the river and/or one or more parking garages. Commercial redevelopment for this area also has potential.



Redevelopment Target Areas Map
2009 Re-examination Report - H2M Group

CURRENT ZONING ALLOWED IN THE REDEVELOPMENT AREA:

The Area of Investigation is located within the B-2 Zoning Classification and is one block east of the Rehabilitation Plan but falls within the Banking, Cultural and Educational Sphere of Influence described in the 2001 Master Plan and 2009 Reexamination Report. The following uses are permitted in the B-2 Zone:

1. All uses permitted in the B-1 District except studios for instruction of self-defense, day nurseries, nursery schools and automatic coin-operated laundries;
2. Appliance stores;
3. Book and stationary stores;
4. Business or vocational schools;
5. Department stores;
6. Dry goods and variety stores;
7. Fully enclosed theaters, concert halls, auditoriums;
8. Furniture stores;
9. Hardware and /or building supply stores;
10. Hobby and craft stores;
11. Hotels subject to the requirements of this chapter. A public dance hall shall be permitted as an accessory use to a hotel only;
12. Movie theaters, bowling alleys, and other indoor amusement facilities;
13. Office equipment establishments;
14. Painting, plumbing and wallpaper stores;
15. Photographic equipment and supply stores;
16. Supermarkets;
17. Telegraphic office, telephone exchange;
18. Retail stores and shops where products to be sold are stored within a fully enclosed building, but specifically excluding pawnshops, the sale of used furniture, appliances, machinery, clothing or other used manufactured goods, including antique shops. Recognized charitable rummage sales for a limited period (2 weeks) are not included in this exclusion;
19. Art gallery, studios for dancing, photography, sculpture or musical instruction;
20. Steam or wet-wash laundries, shirt laundries or any other laundry shall be permitted only as accessory use to a hotel. Dyeing establishments or carpet or rug cleaning establishments are prohibited;
21. Bakery where products prepared are retailed on the premises.
22. Bank or trust company or savings and loan institutions, except drive-in facilities;
23. Club, lodge, meeting hall and social recreation building affiliated with a national or international organization;
24. Child care center;
25. Delicatessen store;
26. Drug stores;
27. Florist shop;
28. Funeral parlors, undertaking establishments,
29. Hardware stores;
30. Multifamily dwellings;
31. Municipal, county, state or federal governmental building, library, park or recreation facility, firehouse;
32. Package liquor store;
33. Personal service establishments, including tailor, dressmakers, shoe repair, barbershop, beauty parlors, nail and hair salons, specifically excluding public garages;
34. Pet shops;
35. Professional offices, business offices, governmental offices, office buildings;
36. Restaurant or tavern complying with the definition of "restaurant"
37. Travel agencies or offices;

USE AND BULK REQUIREMENTS

PERMITTED LAND USES

The following uses are permitted in the Redevelopment Plan:

RESIDENTIAL:

Multi-family, apartments, condominiums, townhomes, residence halls, lofts, and live work studios; over or lining retail, commercial and parking structures;

RETAIL:

Retail stores and shops, restaurants, eating and drinking establishments, bakery, delicatessen, hardware, book and stationery, florists; as freestanding or as liner uses around parking structures;

COMMERCIAL:

General office, medical, physical therapy, professional uses and banks, health clubs, art galleries and studios; as freestanding structures or as a liner around parking structures;

HOTEL / CONFERENCE CENTER:

PARKS & OPEN SPACE:

Parks, plazas, open space including hard, soft and streetscape;

PARKING:

Structured parking as a permitted use serving other uses and users;

Additional Regulations for Permitted Land Uses

1. Any use not stated as a Permitted Use is not allowed in the redevelopment area; Fast food restaurants and drive-thru's are strictly prohibited;
2. The minimum depth of any individual retail or commercial business shall be 40'-0";
3. A minimum of one publicly-accessible park, plaza or open space shall be provided as a part of this project and shall be maintained by the redeveloper. The minimum area for this space shall be 7,500 square foot not including perimeter sidewalks;

ACCESSORY LAND USES

1. Lobbies on the ground floor providing access to residential, office or hotel uses on the upper floors;
2. Parking as an accessory use to a permitted use shall be screened of view from all public streets per the Development and Place Making Design Standards;
3. Loading spaces and docks, recycling and refuse storage areas;
4. Retail, office and service uses shall be permitted provided they are subordinate to the principle permitted use including:
 - a. Management Offices;
 - b. Conference center, meeting rooms;
 - c. Fitness Centers;
 - d. Bank branch, walk-up ATM's;
 - e. Spa, beauty and coffee shops;
 - f. Dry cleaning establishments;
5. Accessory structures shall comply in all respects with the requirements of the principal structure;
 - a. No accessory structure shall be located closer to the street right-of-way line than the required front yard setback of the principal use;
 - b. No portion of an accessory structure shall include living quarters except parking structures as an accessory use may contain residential over the structure;
6. When an accessory structure is attached to the principal structure, it shall comply in all respects with the requirements applicable to the principal structure;
7. No accessory structure shall be constructed or placed on any lot unless the principal structure is first constructed or placed upon said lot, with the exception of parking facilities (surface or structure) which may be constructed prior to the primary use;
8. In no event shall the height of an accessory structure exceed the height of the principal building with the potential exception for parking structures which will be allowed to exceed the primary structure for access only;

DENSITY, AREA, YARD AND HEIGHT REQUIREMENTS:

The following requirements apply to the Redevelopment Area:

1. Minimum Lot Size: 50,000 sf
2. Minimum Dwelling Unit Size: 500 sf
 - a. A maximum of 25% of the units can be between 500 sf and 600 sf.
 - b. A maximum of 10% of the units can be over 1,200 sf. (3 bedroom units are prohibited)
3. Minimum Number of Units: 240 units
4. Maximum Number of Units: 440 units
5. Maximum Number of Phases: The property can be developed in a maximum of two construction phases, so long as parking is included in the first phase of development.
6. Minimum Building Setback:
 - a. The minimum building setback along East Salem Street Extension and Midtown Bridge Road Extension is 12'-0" as measured from the existing or proposed curb (whichever is greater) for buildings 5 stories or less and 18'-0" for buildings 6 stories or more;
 - b. The minimum building setback along Midtown Place or any future public street is 12'-0" feet from the existing or proposed curb (whichever is greater);
 - c. Alleys are not considered public street frontages;
 - d. Utility structures, exhaust air vents, backflow preventers, or other similar devices when located above grade, must be located behind the setback, be screened and cannot be located on either East Salem Street Extension or Midtown Bridge Road Extension;
 - e. Utility structures located below grade may be located within the setback of any street;
7. Building Stepback: For buildings 8 (eight) stories or more a minimum 8'-0" building stepback is required along East Salem Street Extension and Midtown Bridge Road Extension with the exception of architectural elements at building corners.
 - a. Architectural corner elements may not exceed more than 25% of any building facade.
8. Minimum Front/Side/Rear Yard: None (Refer to Setback Requirements)
9. Maximum Retail SF: 25,000 gsf (as primary permitted use)
10. Maximum Commercial SF: 25,000 gsf (as primary permitted use)
11. Maximum Building / Lot Coverage: 90%
12. Maximum Number of Stories/Building Height: 14 Stories (176'-0")
 - a. Height is measured from the average of the top of curb of the two corners of the proposed building. For projects located at a corner lot, the height will be measured from the top of curb of the two corners located along the primary building facade;
 - b. See Section Development / Place-Making Design Standards for all building design requirements.

PARKING STANDARDS:

Parking Requirements for the redevelopment area include:

<i>Use</i>	<i>Parking Ratio</i>
Multi-family:	1.25 sp per dwelling unit
Retail/Commercial/Office:	3.0 sp/1,000 GFA*
Hotel:	0.75 sp/ per room + 1 sp/employee**
Public Parking:	Minimum 200 parking spaces are required to be available for public use. A shared parking analysis can be used to determine the total number of parking spaces required for each use including the minimum required 200 public parking spaces based on a development plan.

Notes:

- * Outdoor seating does not count toward the required parking ratios.
- ** Employee parking is based on the number of employees per shift.

On-Street Parking Spaces:

Parking spaces located along the public street(s) abutting the development may be counted toward the minimum number of parking spaces as required in this redevelopment plan.

Shared Parking:

Joint use of up to 50% of the required parking spaces may be permitted for two or more uses located on the same or adjacent parcels, provided the development can demonstrate the uses will not substantially overlap in hours of operation, or in demand for the shared spaces. The shared parking analysis must demonstrate the shared parking plan is feasible based upon a "Shared Parking Analysis" using Urban Land Institute's (ULI) Shared Parking Software or a comparable software model. The Shared Parking Analysis shall be prepared by a credible expert, such as an experienced parking or land use consultant, planner, architect or engineer.

Setback Maneuvering:

No surface parking or maneuvering space is permitted in any required setback, or between the permitted use and the required setback, except driveways providing access to the parking area may be installed across these areas.

Bicycle Parking:

Bicycle parking is required in publically accessible areas for new developments at one bicycle parking space for every 50 vehicle parking spaces, up to 200 vehicle parking spaces.

- a. Thereafter, one bicycle parking space shall be provided for every 100 vehicle parking spaces. Fractions equal to or greater than one half shall be considered to be one bicycle space;

Handicapped Accessible Parking Requirements:

Refer to Section 175-10.01C Handicapped Parking Schedule in the City of Hackensack Zoning Ordinance.

Additional Off-Street Parking Regulations:

For all parking standards not covered in this section refer to Section 175-10.2 Additional Off-Street Parking Regulations A through K and Section 175-10.5 Design Criteria A through H of the City of Hackensack Zoning Ordinance with the exception of the following:

- a. Off-street parking dimensions can be 8'-6" x 18'-0";
- b. Two lane drive aisle dimensions can be 23'-0";
- c. Parallel parking dimension shall be a minimum 22'-0" x 8'-0".

CONCEPTUAL SITE PLANS



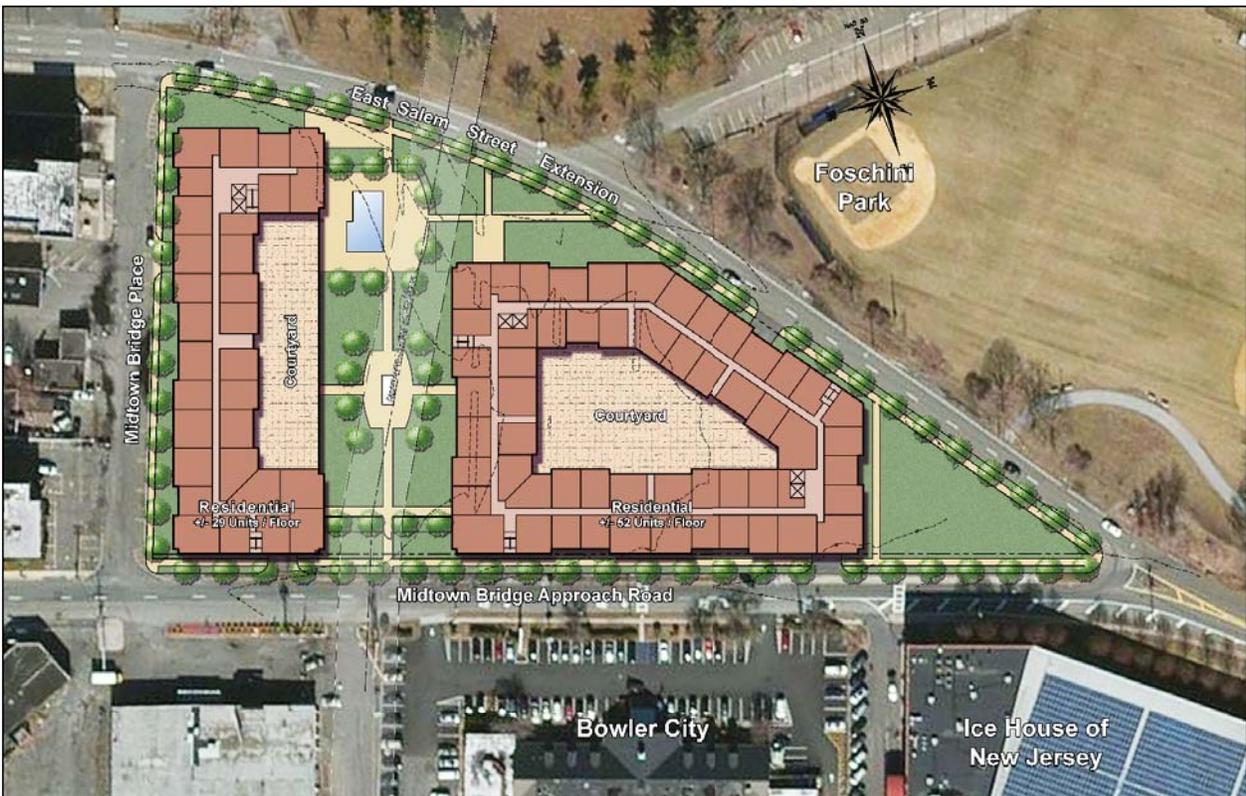
Redevelopment Area Conceptual Site Plan:
Parking Level



Redevelopment Area Conceptual Site Plan:
Parking Level with Retail at the corner of Midtown Bridge Approach Road and East Salem Street Extension



Redevelopment Area Conceptual Site Plan:
Residential Levels with a public street located between the two buildings over the BCUA Easement.



Redevelopment Area Conceptual Site Plan:
Residential Levels with a common courtyard over the BCUA Easement

DEVELOPMENT / PLACE-MAKING DESIGN STANDARDS

The design standards in this section provide the criteria for the redevelopment area. Any future development is subject to these provisions and shall be built in accordance with the minimum design standards specified in this section.

Building Architectural Materials & Character:

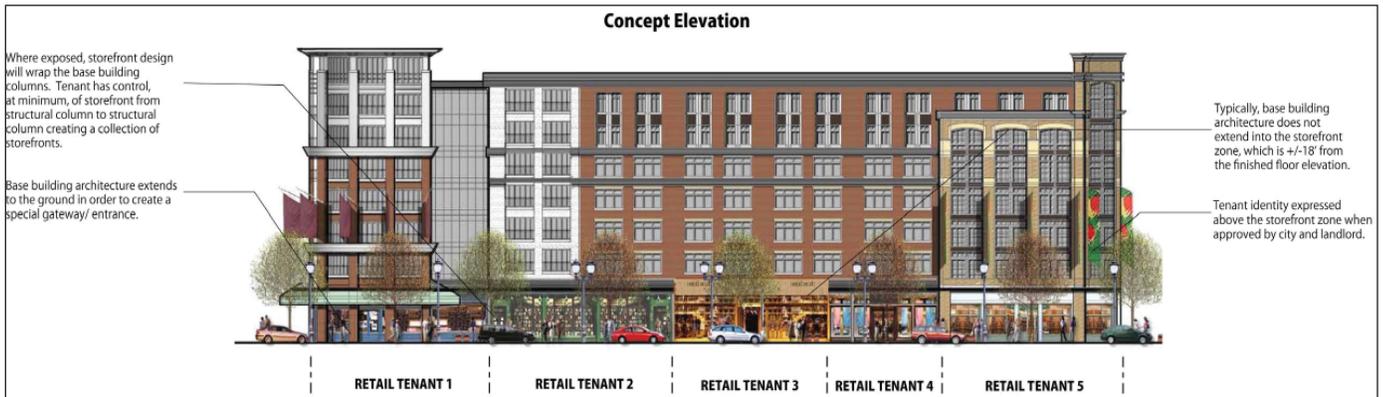
1. All buildings shall be constructed with high quality materials that reinforce pedestrian scale;
2. Primary building materials shall include: brick, and/or glass shall cover a minimum of 75 percent of each building façade, with accent materials comprising a maximum of 25% for each building façade;
 - Vinyl siding of any type or grade is strictly prohibited on any portion of building façade in the redevelopment area;
 - Pitched roofs are strictly prohibited on any portion of the building(s).
3. The base of buildings shall be distinguished from the remainder of the building with an emphasis on providing design elements that will enhance the pedestrian environment particularly at the street level;
4. Elements such as cornices, belt courses, corbelling, molding, string courses, ornamentation, changes in material or color, and other sculpturing of the base are appropriate and should be provided to add special interest;
5. Special attention must be given to the design of windows at the base of buildings. Ribbon windows are strongly discouraged. Recessed windows that are distinguished from the shaft of the building through the use of arches, pediments, mullions, and other treatments are encouraged;
6. Buildings shall be broken up vertically into a base, a middle and a top



Multi-family residential building typ.



Mid rise multi-family residential building typ.



Conceptual Building Design with unique first floor design to provide variety at the streetscape



Distinct residential entrance



Structured parking facade screening

and horizontally into bays, through the use of building articulation or change in materials. The base should not extend beyond the first three levels. Material changes should occur at logical points delineated by a change in the building or cornice. For buildings in excess of 120'-0" in length a vertical demarcation should be included at a minimum of every 60'-0" of building façade;

7. Any retail, commercial or office storefront design at the first level should reflect the individual tenant's brand identity;

Building Orientation:

1. All buildings shall be oriented toward public streets and public open spaces. The building front is considered to be the elevation facing any public street unless the building is adjacent to a park, plaza or open space, in which case the building should orient to both the street and the open space;
2. The first floors of all buildings, including structured parking, must be designed to encourage and complement pedestrian-scale and activity;
3. Ground floor retail shall have a 16'-0" minimum floor to floor height;

Building Entrances:

1. Building entrances should be easily identifiable with unique and interesting signage for each individual use;
2. Retail entrances can have up to a 4'-0" pop-out / protrusion / recess into the 18'-0" setback or adjacent public open space for columns or other architectural features that distinguish the commercial or retail



Retail and outdoor seating with residential above adjacent to a public open space



Structured parking facade with residential above



Structured parking facade screening



Canopies and balconies for residential

storefront;

- Entrances for residential uses should be separate and distinct from other uses;

Street Level Frontage / Uses:

- Residential, retail and/or commercial uses are strongly encouraged along the East Salem Street and Midtown Bridge Approach;
- Parking structures are permitted at the street level and would be required to meet the design requirements outlined in this section;
- A public park / plaza / gateway is strongly encouraged at the intersection of East Salem Street and Midtown Bridge Approach with retail or commercial fronting it as this intersection represents a gateway into Hackensack.

Structured Parking:

- The exterior facade of any parking structure that fronts on the a public street shall be designed using the same materials and opening sizes and locations as the rest of the building which include decorative architectural details such as brick, glass, accent materials, grillwork or glazing. These elements shall be consistent with the rest of the building facade including similar size and scale to the residential windows. Except for at parking entrances no vehicles shall be visible from the center line of any public street.
- Ramps shall not be visible from East Salem Street Extension or Midtown Bridge Approach;
- Cabling or exposed concrete alone does meet the screening requirement;
- Driveway and garage openings should not exceed 24'-0" in width and should include traffic calming measures and a change in surface materials where driveways cross and sidewalk to help ensure pedestrian and bicycle safety;

Canopies and Balconies:

- Canopies, awnings, and similar architectural accents are encouraged at entrances to buildings and in open space areas. Such features should be constructed of rigid or flexible material designed to complement the tenant's identity at the street level.
- Any canopy may extend from the building up to one half of the width of the setback area in front of the building, or eight (8) feet, whichever is less;



Canopies and balconies for residential



Architectural building character with base, middle and top.

3. Ground supports are not permitted in the minimum setback, sidewalk or in the public right-of-way;
 - These features should not extend over or interfere with the growth or maintenance of any required tree plantings;
4. The minimum overhead clearance shall be ten (10'-0") feet. If a canopy, awning, cornice, or other appurtenance extends into the public right-of-way, an encroachment agreement shall be required;
5. Balconies/balconettes can be incorporated into the building façade and are allowed to project into the building setback up to 4'-0" starting at the second level;

Mechanical Equipment Screening:

1. Screening of rooftop mechanical equipment is required.
2. All rooftop mechanical equipment including cell phone antennae shall be screened from view from all adjacent public streets, open spaces and parks in all directions and elevations to minimize the negative impact;
3. Screening materials shall be consistent with the architectural detail, color and materials of the building; Wire mesh screening is not permitted;
4. All roof and HVAC systems must meet the building code requirements and at minimum be set back from the building edge a minimum of 15' from any street or public open space and screened as to not be visible from any adjacent public street or public property;
5. Any wall pack ventilation unit facing a public street shall match the adjacent building material color;

Building Service Locations:

- a. All service locations for the redevelopment area shall be provided at the rear or side of the building and shall not be permitted directly from East Salem Street Extension;

Doors and Windows:

1. Where expanses of solid wall are necessary, they may not exceed 30'-0" feet in length without architectural enhancements;
2. The first floor and street level shall be designed to address all public streets and all open space areas;
3. For buildings fronting two streets or one street and a public open

space the entrance should provide access from the primary street and is encouraged to provide access from the secondary street or public open space;

4. For buildings that do not provide access from the secondary street or open space, the building facade should provide windows and/or architectural details that are aesthetically interesting;
5. For retail uses operable windows are encouraged;
6. No development subject to these provisions may have exterior walls with a reflectivity value in excess of 35 percent;

Openings / Ventilation:

1. Any openings for ventilation, service, or emergency access located at the first floor level must be decorative and must be an integral part of the overall building design;
2. Openings as well as pedestrian and vehicular entrances must be designed so that cars parked inside are not visible from the street;



Streetscape with landscape planting areas



Streetscape with pedestrian scaled lighting and crosswalks



Streetscape with pedestrian scaled lighting

STREETSCAPE DESIGN STANDARDS:

A successful neighborhood is not complete until its parks, open space, sidewalks and streetscape have been designed and “furnished”. The items that are referred to as streetscape elements include street lights, sidewalks, benches, trash receptacles and other street furniture which reinforce the character of the street and the neighborhoods. The following section addresses an overall approach for the design of streetscape elements that are to be considered as part of the redevelopment plan. These standards are meant to establish the minimum criteria that will be required for the design and implementation of streetscape, parks and open space improvements.

Street Trees:

- Street trees shall be planted in either grates or open landscape areas equivalent to 30’ on center along all public street frontage;
- For buildings located adjacent to public open spaces, parks or plazas, trees may be planted in open grass areas;
- Above ground planters may be considered to fulfill the street tree requirement only in situations where underground utilities prevent tree pits;
- Street trees shall be planted with a minimum six inch (6”) caliper, shall be trimmed up to 8’-0” and shall be in accordance with the “American Standard for Nursery Stock” published by the American Association of Nurserymen;
- A single street tree species should be installed for all existing and new public streets and should be based on the City of Hackensack Approved Planting List.

Street Lighting:

- Street lights shall be the City of Hackensack city standard pedestrian scale light pole and fixture;
- Street lights shall be shielded from second floor uses and shall be a maximum of 16’ in height, located on center between street trees, at a maximum of 90’-0” on center (Photo-metrics shall be provided to determine the final street light spacing);
- Street light specifications and locations shall be submitted for review and approval prior to installation;
- Street lights should be located as part of the streetscape and function as a unifying element of other streetscape items including trees, benches and paving;



City of Hackensack:
Typical pedestrian scale street light fixture



City of Hackensack:
Typical bench



Streetscape with outdoor seating and grates

- e. Building light fixtures should exhibit an aesthetic as well as functional purpose to create interest and a sense of scale for the pedestrian;
- f. LED Lighting is encouraged;
- g. Luminaires should be translucent or glare-free using opaque glass or acrylic lenses;
- h. Diffusers and refractors should be installed to reduce unacceptable glare; particularly adjacent to residential uses;

Sidewalks:

- a. Sidewalks should be a minimum of 6'-0" and may consist of brick, concrete or a combination of the two and should be consistent for the all public streets in the redevelopment area;
- b. Asphalt of any type or grade is not permitted as a sidewalk material;

Seating, Benches:

- a. Seating should be grouped together as much as possible and placed in public open spaces, plazas or gathering places;
- b. Seating should be designed and placed appropriately to provide an amenity to the public;
- c. Seating should not obstruct views for vehicles, sidewalks for pedestrians, or signs and displays for businesses;
- d. The type of bench will be determined during the design phase and can vary along Main Street so long as the same bench is used for a minimum of two block lengths and for both sides of the street;

Planting Pots & Planters:

- a. Pots and planters can be used particularly in areas with retail as a complement to the surrounding streetscape and are permitted in the setback with review and approval of the Building Department;
- b. Pots and planters can be placed as a separation between pedestrians and vehicles;
- c. Pots and planters are ideally located near seating areas, but plant materials should not interfere with circulation or comfort;

Tree Grates:

- a. Tree grates are permitted and can be used wherever a tree is placed within a high traffic area;
- b. Tree grates must allow for tree growth and be made of ductile iron and should be factory painted;
- c. Electrical outlets should be provided within the grate area to allow for lighting opportunities and should be designed to support up-lighting;



City of Hackensack trash and recycling receptacle



Bicycle rack



Bollards with lighting

Trash Receptacles:

- a. Trash receptacles should be located conveniently for pedestrian use and service parks, plazas and other public gathering places.
- b. Trash receptacles should be permanently attached to deter vandalism and have sealed bottoms with sufficient tops to keep contents dry and out of pedestrian view;

Bicycle Racks:

- a. Bicycle racks should be permanently mounted and placed in convenient public locations to encourage bicycle use;
- b. Bicycle racks should be placed so as not to obstruct views or cause hazards to pedestrians or drivers;

Bollards:

- a. Bollards should integrate with and aesthetically complement the overall streetscape concept;
- b. Bollard design should respond to the area it supports (Plaza vs Services);
- c. Bollards may be chained or cabled together to ensure pedestrian safety or define areas for public functions;

Fountains:

- a. Fountains or water features are encouraged to be located in key public areas such as parks or plazas and should allow ample pedestrian circulation on all sides;
- b. Fountains or water features should be designed to encourage "sitting" along the edges or interaction from the public;

Public Art:

- a. Public art should be "accessible"; tie to the history of Hackensack and if possible, should be created by a local source;
- b. Public art may include water, seating, planting, decorative architectural elements or plaza space as part of the design;
- c. Public art should be visible, but not interfere with pedestrian circulation or create a traffic hazard and should be made of durable, weatherproof materials and should be crafted, not "sharp";

Kiosks:

- a. Kiosks should be used for retail purpose or to impart community information to the public and should be positioned to complement and respect other street furnishings such as benches and lighting;
- c. Kiosks should be accessible and attractive from all sides and well-illuminated and should be flexible to allow for up-to-date information;



Public Sculpture: "Alice in Wonderland"



Flag banner signs

Parking Meters:

- a. On-street parking meters are required along any existing and new public street in the redevelopment area;
- b. Parking meter specifications and locations shall be determined by the City of Hackensack;

Utility Accessories:

- a. Utility boxes, meters, manhole covers and fire hydrants should be coordinated with other streetscape accessories;
- b. Utilities should be readily accessible and placed so as not to obstruct visibility and movement;
- c. Utility locations should minimize visual and physical impact as much as possible and should blend in with the surroundings or enhance the area;

Public Plaza / Open Space:

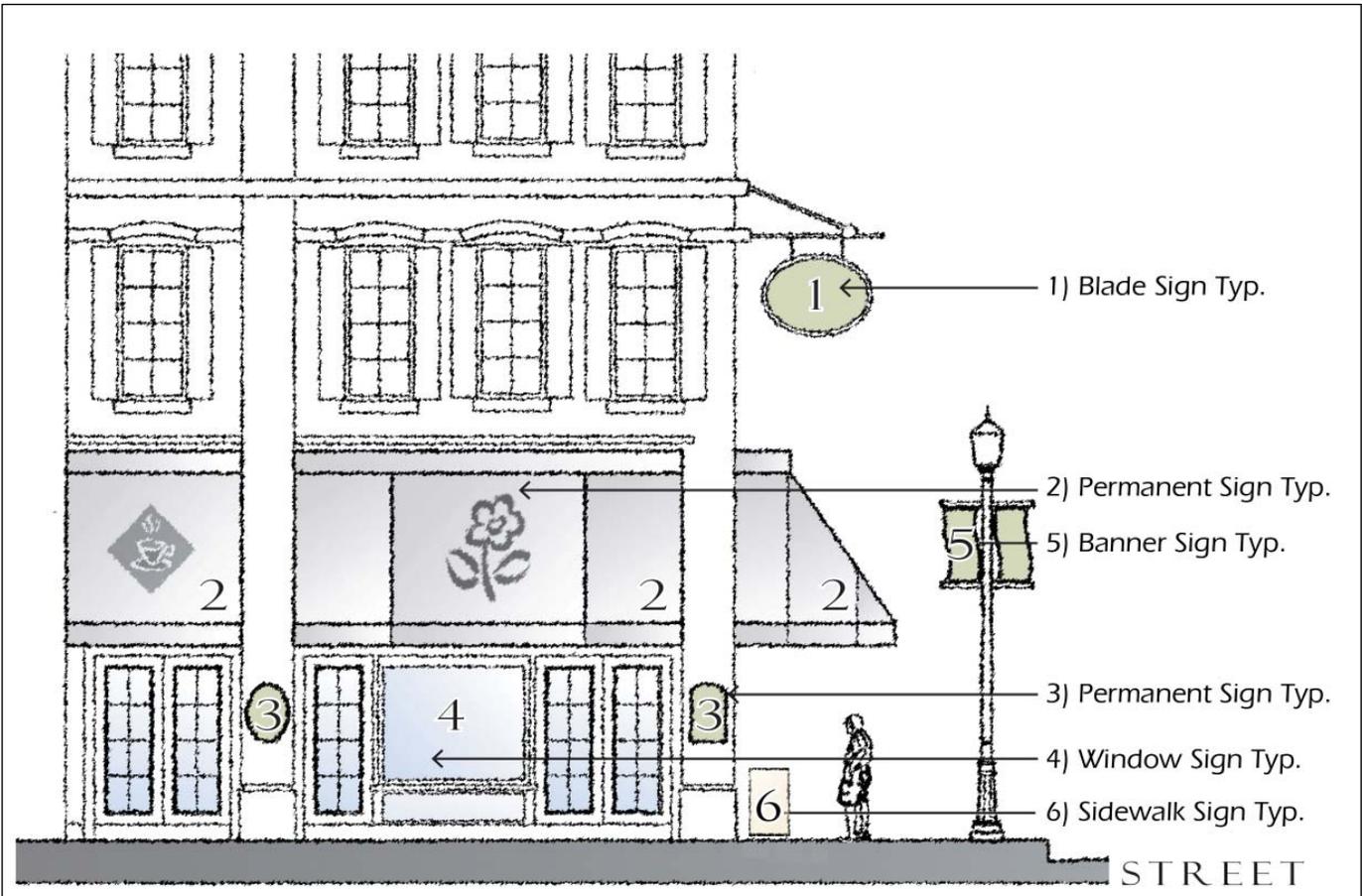
- a. A minimum of one publicly-accessible park, plaza or open space shall be provided and maintained as a part of this project with a minimum of 7,500 square foot not including perimeter sidewalks;
- b. The intersection of East Salem Street and Midtown Plaza Approach represents the preferred location for public open space;
- d. Open spaces should be safe, well lit, provide public seating, and include specialty paving (moveable tables and chairs are strongly encouraged);
- e. Public open spaces should consider including a gateway or other public art as a component of the design;



Blade sign

SIGNAGE STANDARDS:

- a. Projects are encouraged to explore a variety of signage types, sizes and styles with the objective of integrating the design of the signage into the overall project.
 - i. The goal is to maintain creative consistency that identifies the project and potential tenant's identity;
 - ii. Emphasis should be on durable, natural materials and quality manufacturing and can include cast, polished or painted metal; painted, stained or natural wood; glazed and ceramic tile; etched, cut, edge-lit or stained glass; cast stone and carved natural stone;
- b. For all signage standards refer to City of Hackensack Rehabilitation Plan and Section 175-7.14 SIGNS: A through M of the City of Hackensack Zoning Ordinance.



Signage locations typ.

RELATIONSHIP OF REDEVELOPMENT PLAN TO MASTER PLANS

State Development and Redevelopment Plan:

In reviewing the New Jersey State Development and Redevelopment Plan Volumes 1 - 4 the following information pertains to goals and policies for a program of rehabilitation which discuss the development and redevelopment policies for urban areas.

Volume II – State Plan Goals and Policies:

a. Revitalize the State’s Cities and Town Centers:

“Revitalize New Jersey’s cities and towns by investing wisely and sufficiently in improvements in their infrastructure systems, public spending programs, tax incentives and regulatory programs to leverage private investment and to encourage infill and redevelopment in ways that are consistent with the State Plan’s vision and goals.”

b. Conserve the State’s Natural Resources and Systems:

“Conserve the State’s natural resources and systems by planning the location and intensity of growth to maintain natural resource and systems capacities and make the necessary infrastructure investments to protect natural resources and systems in ways that guide growth and development in ways that are consistent with the State Plan’s vision and goals.”

c. Promote Beneficial Economic Growth:

“Promote beneficial economic growth in locations and in ways that improve the quality of life and the standard of living for all New Jersey residents by providing infrastructure in advance of, or concurrent with, the impacts of new development sufficient to maintain adequate facility standards, by encouraging partnerships and collaborative planning with the private sector and by capitalizing on the State’s strategic location, economic strengths including its existing business enterprises, entrepreneurship, the research and development capacity of its institutions of higher learning, skilled workforce, cultural diversity and logistic facilities in ways that are consistent with the State Plan’s vision and goals.”

d. Protect the Environment, Prevent and Clean up Pollution:

“Protect the environment, prevent and clean up pollution by planning for growth in compact forms at locations, densities and intensities that protect land, air and water quality, allow expeditious regulatory reviews and encourage multi-modal transportation alternatives to the automobile to help achieve and maintain acceptable air quality standards.....”

e. Provide Adequate Public Facilities and Services at Reasonable Cost:

The State Plan Policy Map (SPPM) Planning Areas

Objectives as applied to the different Planning Areas ensure an appropriate balance between development and conservation of natural resources as directed by the State Planning Act and as guided by the State Plan’s vision and goals. If adhered to, the differing Statewide Policy Objective will also ensure that the appropriate development will occur within Planning Areas’ centers and environs. Infrastructure capacities and natural systems were important criteria in identifying and delineating the Planning Areas, Centers and Environs classifications.

The State Plan’s vision and goals considers five Planning Areas and two sub-Planning Areas. The Five Planning Areas are the following:

- The Metropolitan Planning Area** – This Planning Area includes a variety of municipalities that range from large Urban Centers to 19th century towns shaped by commuter rail and post-war suburbs. The Communities in this Planning Area have strong ties to major metropolitan centers – New York/ Newark/Jersey City metropolitan region; the Philadelphia/Camden/Trenton Metropolitan Region; and on a smaller scale the Eastern/Pittsford Metropolitan Region. These municipalities have many things in common: mature settlement patterns; infrastructure systems that are approaching their reasonable life expectancy; an aging housing stock in need of rehabilitation; recognition that redevelopment will be the predominant form of growth; and a growing realization of the need to regionalize services and systems. In addition, the wide and often affordable choice of housing in proximity to New York and Philadelphia has attracted significant immigration, resulting in noticeable changes in demographic characteristics over time.

In the Metropolitan Planning Area, the State Plan’s intent is to do the following:

- Provide for much of the State’s future development and redevelopment
- Revitalize Cities and Towns
- Take advantage of increased densities and compact building design
- Encourage distinctive, attractive neighborhoods with a strong sense of place
- Provide for mixed-use concentrations of residential and commercial activity
- Create a wide range of residential housing opportunities and choices with income mix
- Provide for a variety of multi-modal transportation alternatives
- Prioritize clean-up and redevelopment of brownfields and greyfields sites
- Create cultural centers of state-wide significance
- Re-design any existing areas of low-density sprawl

- The Suburban Planning Area** – The Suburban Planning Area is located adjacent to the Metropolitan Planning Area, but can be distinguished by a lack of high intensity Centers, by the availability of developable land, and by a more dispersed and fragmented pattern of predominantly low-density development. Suburban Planning Areas are served by regional infrastructure. These areas have typically been designated for growth in municipal master

30 New Jersey State Development and Redevelopment Plan DRAFT/FINAL PLAN

New Jersey SDRP - The State Plan Policy Map “Metropolitan Area 1”

The State Plan Policy Map (SPPM)

POLICY Map of the Draft Final New Jersey State Development and Redevelopment Plan

20 New Jersey State Development and Redevelopment Plan DRAFT/FINAL PLAN

New Jersey State Development and Redevelopment Plan Policy Map

“Provide adequate public facilities and services by supporting investments based on comprehensive planning and by providing financial incentives for jurisdictions that cooperate in providing public infrastructure and shared services. Encourage the use of infrastructure needs assessments and life-cycle costing. Provide adequate public facilities in ways that are consistent with the State Plan’s vision and goals.”

- f. Provide Adequate Housing at a Reasonable Cost:
 “Provide adequate housing at a reasonable cost through public/private partnerships that create and maintain a broad choice of attractive, affordable, ecologically designed housing, particularly for those most in need. Create and maintain housing in the Metropolitan and Suburban Planning Areas and in Centers in the Fringe, Rural and Environmentally Sensitive Planning Areas, at densities which support transit and reduce commuting time and costs, and at locations easily accessible, preferably on foot, to employment, retail, services, cultural, civic and recreational opportunities. Support regional and community-based housing initiatives and remove unnecessary regulatory and financial barriers to the delivery of housing at appropriate locations.”
- g. “Preserve and Enhance Areas with Historic, Cultural, Scenic Open Space, and Recreational Value – Preserve, enhance, and use historic, cultural, scenic and recreational assets by collaborative planning, design, investment and management techniques. Locate and design development and redevelopment and supporting infrastructure to improve access to and protect these sites. Support the important role of the arts in contributing to community life, civic beauty and redevelopment in ways that are consistent with the State Plan’s vision and goals.”
- h. “Ensure Sound, Coordinated and Integrated Statewide Planning – Ensure sound, coordinated and integrated statewide planning by using the State Plan as a guide to planning and growth related decisions at all levels of government in ways that are consistent with the State Plan’s vision and goals.”
- i. “Increase Energy Efficiencies and Reduce Greenhouse Gas Emissions – Increase Energy Efficiencies and Reduce Greenhouse Gas Emissions by promoting the improved coordination and integration of transportation planning and land-use planning and decision-making to reduce vehicle miles traveled (VMT); and by the citing, development, design and use of green-building construction materials and techniques in ways that are consistent with the State Plan’s vision and goals.”

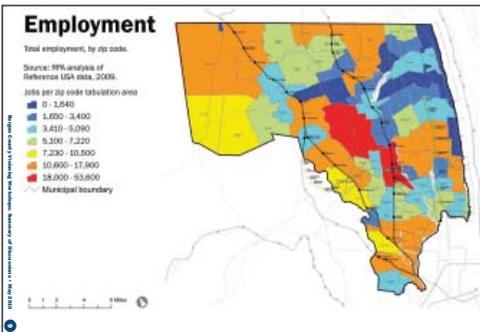
The State Planning Act (N.J.S.A. 52:18A-196 et. seq.):

- a. “Among the goals of the act is the following: ...conserve its natural resources, revitalize its urban centers, protect the quality of its environment, and provide needed housing and adequate public services at a reasonable cost while promoting beneficial economic growth, development and renewal.”
- b. “It is in the public interest to encourage development, redevelopment and economic growth in locations that are well situated with respect to present or anticipated public services and facilities, giving appropriate priority to the redevelopment, repair,

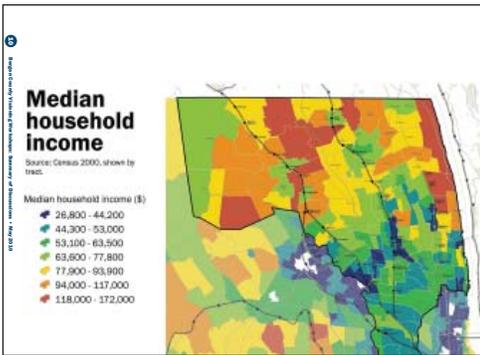
rehabilitation or replacement of existing facilities and to discourage development where it may impair or destroy natural resources or environmental qualities that are vital to the health and well-being of the present and future citizens of this state.” (N.J.S.A. 52:18A-196 (d))

The State Plan Policy Map (SPPM)

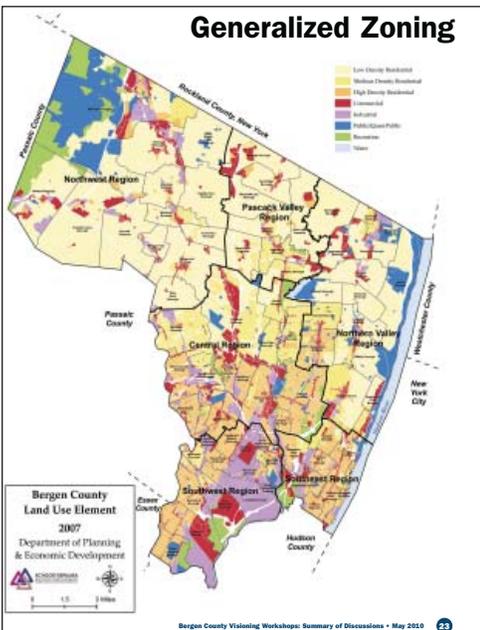
- a. Recognizes that New Jersey requires different approaches in its Metropolitan, Suburban, Rural and Environmentally Sensitive Planning Areas. The New Jersey State Development and Redevelopment Plan adopted by the State Planning Commission, places the City of Hackensack in Planning Area 1 - the Metropolitan Planning Area and states:
- b. The Metropolitan Planning Area:
 “This Planning Area includes a variety of municipalities that range from large Urban Centers to 19th century towns shaped by commuter rail and post-war suburbs. The Communities in this Planning Area have strong ties to major metropolitan centers – New York/Newark/Jersey City metropolitan region; the Philadelphia/Camden/Trenton Metropolitan Region; and on a smaller scale the Easton/Phillipsburg Metropolitan Region.”
- c. “These municipalities have many things in common: mature settlement patterns; infrastructure systems that are approaching their reasonable life expectancy; an aging housing stock in need of rehabilitation; recognition that redevelopment will be the predominant form of growth; and a growing realization of the need to regionalize services and systems. In addition, the wide and often affordable choice of housing in proximity to New York and Philadelphia has attracted significant immigration, resulting in noticeable changes in demographic characteristics overtime.”
- d. “In the Metropolitan Planning Area, the State Plan’s intent is to do the following:”
 - “Provide for much of the State’s future development and redevelopment;”
 - “Revitalize Cities and Towns;”
 - “Take advantage of increased densities and compact building design;”
 - “Encourage distinctive, attractive neighborhoods with a strong sense of place;”
 - “Provide for mixed-use concentrations of residential and commercial activity;”
 - “Create a wide range of residential housing opportunities and choices with income mix;”
 - “Provide for a variety of multi-modal transportation alternatives;”
 - “Prioritize clean-up and redevelopment of brownfields and greyfields sites;”
 - “Create cultural centers of state-wide significance;”
 - “Re-design any existing areas of low-density sprawl;”



Draft Bergen County Master Plan
Employment Data



Draft Bergen County Master Plan
Medium Household Income



Draft Bergen County Master Plan
General Zoning

Bergen County Master Plan:

In reviewing the Final Draft Bergen County Master Plan the following information pertains to goals and policies for a program of rehabilitation which discuss the development and redevelopment policies for urban areas.

Bergen County Master Plan Overview:

- a. "The Bergen County Department of Planning and Economic Development is undertaking a county-wide planning effort that will result in the first County Master Plan in some time."
- b. "The Master Plan will create a unifying vision for the County's 70 municipalities and help them plan for sustainable growth while protecting environmental resources. Development of the Master Plan will be a collaborative process involving the County, municipalities, regional agencies, public and private sector stakeholders and Bergen County citizens."

Draft Report

- a. The Draft Report provided on the Bergen County Master Plan website includes the following descriptions:
 - "Future growth will primarily occur through redevelopment and infill;"
 - "Redevelopment provides opportunities to create new public spaces and green areas in places that have none;"
 - "Redevelopment on a large scale using green guidelines has the potential over time to significantly ameliorate many of the county's storm water run-off and water quality issues;"
 - "Redevelopment can occur spontaneously and be privately driven, or it can take place as a result of a public initiative, usually under the jurisdiction of a local redevelopment agency;"

NEIGHBORING COMMUNITY MASTER PLANS:

Information for the Master Plans of the adjacent municipalities have been included as stated in the 2009 Master Plan Reexamination Report which includes the following information:

Borough of Bogota:

“The 2003 Master Plan for Bogota recommends a rezoning of properties located in the Planned Development zone. This zone directly abuts the City’s southeast boundary. The planned development zone has existed since the last master plan was adopted and no planned development has occurred on these parcels for the past ten years. Currently, these properties contain a driving range and have been there for a long time. The Borough’s master plan therefore recommends that this area be zoned for 1-2 zones, which permits light-industry use, warehouses, and offices, indoor and outdoor recreational use.”

Borough of Hasbrouck Heights:

“The 2003 Master Plan Reexamination Report recommends that the Borough should consider redeveloping portions of lands along Route 17, which connects Hasbrouck Heights to Hackensack City. However, these projects, if and when they are built, will not significantly impact Hackensack.”

Borough of Little Ferry:

“The Borough’s 2004 Master Plan Reexamination Report recommends significant redevelopment along the waterfront. The plan recommends developing the area around Bergen Turnpike and Valley Road intersection that currently contains an underutilized shopping center to be redeveloped to include hotel, high-end large retail anchors, restaurants and a mix of offices. The plan recommends redevelopment along the waterfront that would include low-rise (1-3 story) mixed-use development with an array of recreational amenities and pedestrian walkways along the riverfront. The recommended projects are consistent with the development currently occurring along River Street in Hackensack.”

Borough of Lodi:

“The Borough of Lodi’s 2010 Master Plan Reexamination Report” does not have any significant plans to areas that border Hackensack which would affect Hackensack.

Borough of Maywood:

“The Borough of Maywood 2003 Master Plan recommends that redevelopment study should be undertaken for areas along the southwesterly side of the Borough that have access from Route 17. This, however, will not impact Hackensack.”

Borough of Paramus:

“The 2005 Master Plan Reexamination Report recommends that the Borough should evaluate for any areas in need of redevelopment within the Borough. The Plan also suggests rezoning certain residential properties that directly abut the Maywood Borough boundary. However, this will not impact Hackensack.”

Borough of River Edge:

“The Borough does not have any significant plans that would affect Hackensack.”

Borough of South Hackensack:

“The Borough prepared a land use element of the Master Plan in 2001; however, no changes are proposed at this time.”

Borough of Teterboro:

“The 2006 Master Plan Reexamination Report recommends that the Borough should evaluate potential properties within the existing industrial area that are in need of redevelopment.”

Township of Teaneck:

“The Township recently adopted its Master Plan, April 2, 2007. The plan recommends that some properties located centrally within the Township be rezoned. However, the proposed zone changes will not impact the City of Hackensack.”

Village of Ridgefield Park:

“Portions of the industrial area that abut the Hackensack boundary to the east are being redeveloped. As part of the riverside park system plan, a new bike path is proposed along the river. These improvements are consistent with the waterfront redevelopment projects that Hackensack and Bergen County are proposing.”