

Record Site Redevelopment Plan



Block 203.01 - Lots: 2, 3, 3.01

Block 300 - Lots 3.01 & 3.02

CITY OF HACKENSACK, BERGEN COUNTY, NEW JERSEY

September 2015

Prepared by:

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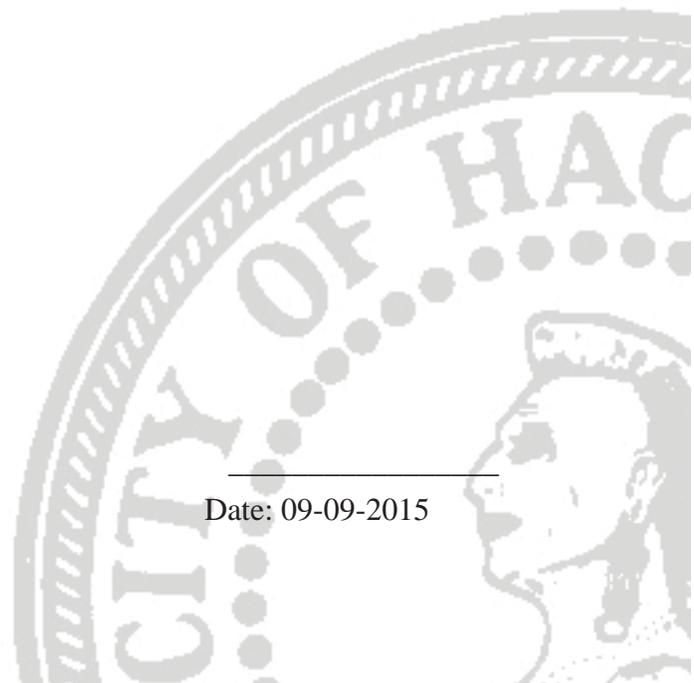
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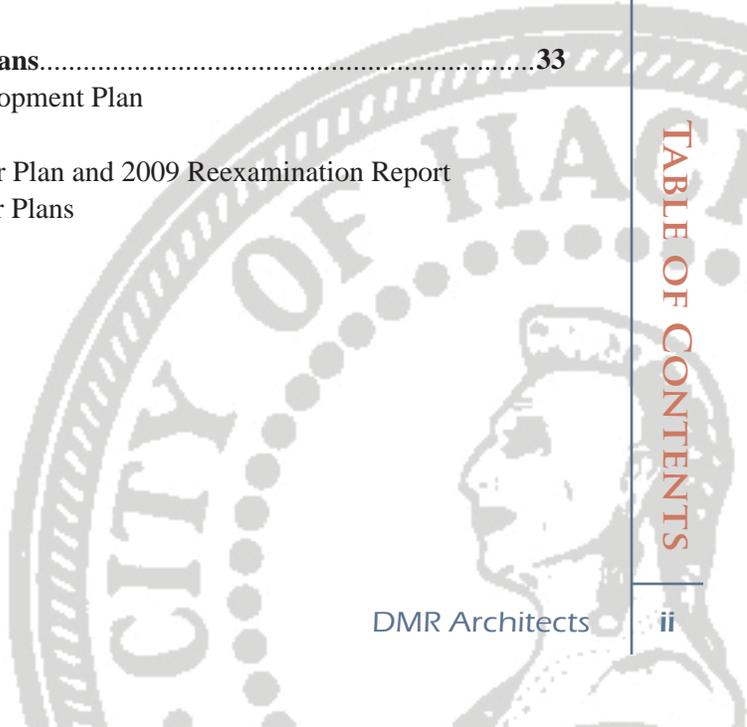
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BACKGROUND INFORMATION

The City of Hackensack is in the midst of a renaissance. Over the past few years the City has taken tremendous steps in implementing and adopting new zoning through the creation of a Rehabilitation Plan which provides a clear vision to transform the downtown into a mixed use, pedestrian friendly environment.

The Plan which was adopted in 2012 promotes:

1. Smart growth principles by creating zoning which increases development flexibility, reduces parking ratios and promotes mixed-use, pedestrian friendly development in the downtown;
2. Connectivity to existing public infrastructure, including the two NJ Transit Rail Stations, the NJ Transit Regional Bus Station and Routes 4, 17, 46, Interstate 80 and the Garden State Parkway;
3. A mixture of uses with a variety of residential housing options to encourage walkability and active streetscapes;
4. Redevelopment and rehabilitation through architectural, neighborhood design standards that ensure high quality development;
5. The implementation of a two way street system; and
6. Strategies which include municipal tools and mechanisms to promote revitalization.

In only a short period of time since the adoption of the plan, the City has already begun to see the benefits of these strategies. For the first time in over thirty years, a significant residential development of approximate 220 units is under construction. In addition, the City has seen a wave of developer interest throughout the rehabilitation area with several other mixed use projects that are either in the design or entitlement process.

Hackensack represents a premier location in Bergen County that supports higher density mixed use residential development. The City has two New Jersey Transit Stations which run directly to Hoboken as well as the regional New Jersey Bus Station that provides direct access throughout the county and to New York City. The downtown has access to Routes 4, 17, 46, Interstate 80 and the Garden State Parkway and is located eight miles from the George Washington Bridge, and 13 miles from the Lincoln Tunnel.

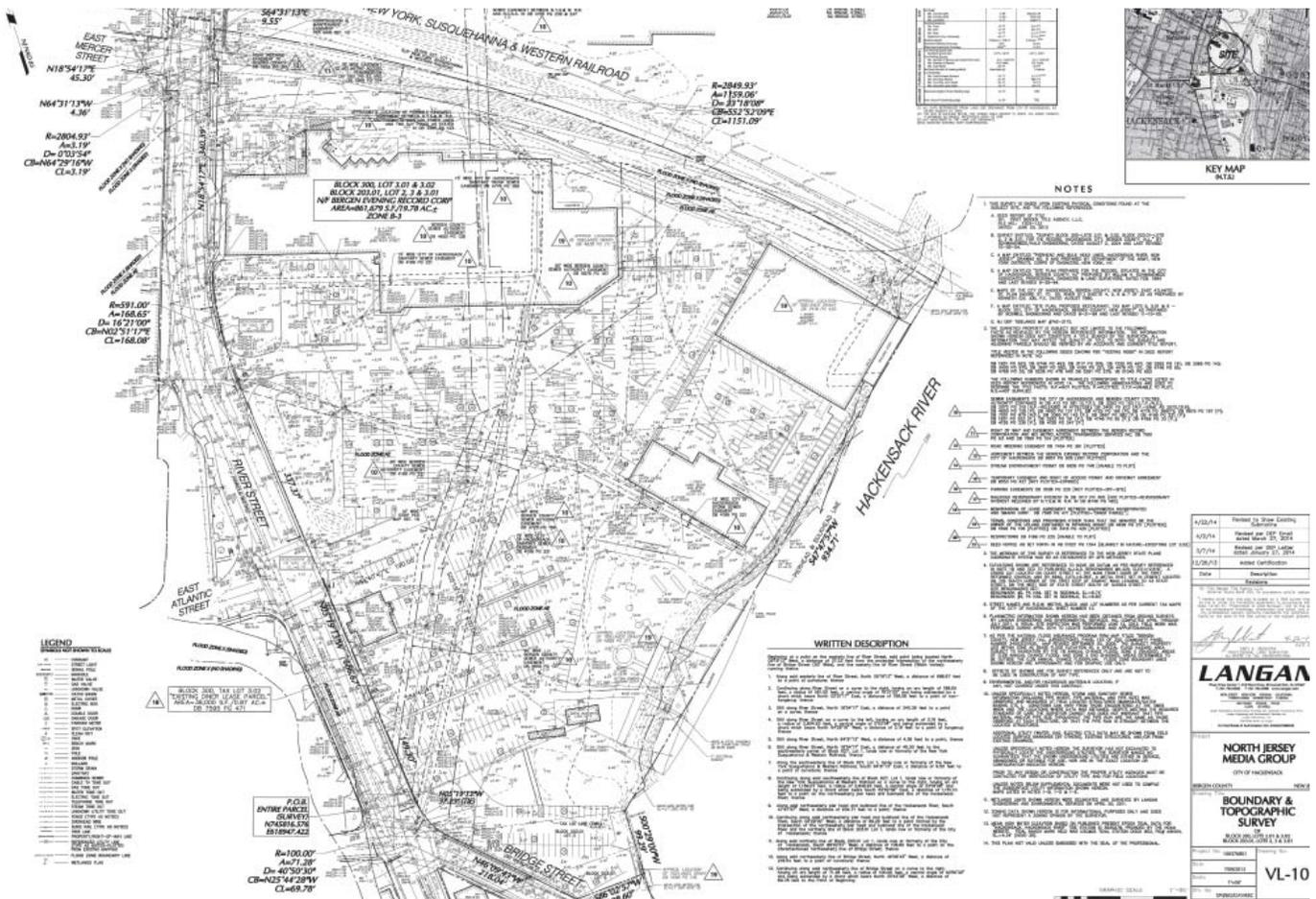
With over 43,000 residents and a day time population estimated at over 100,000 the City is home to the two largest employers in Bergen County including the Hackensack University Medical Center and the County Administration Offices. With the recent announcement of a partnership between Georgetown University and the

INTRODUCTION

BASIS FOR THE PLAN:

This redevelopment plan has been prepared for Block 203.01 – Lots 2, 3 and 3.01, Block 300 – Lots 3.01 & 3.02. The addresses for the plan include: 80 & 150 River Street, along with 62 & 76 Bridge Street . The approximately 19.7 acre site is located in the Downtown Rehabilitation Area between Bridge and River Streets.

The City of Hackensack Mayor and Council in 2015 determined by resolution this property qualified as an “area in need of redevelopment” in accordance with the criteria specified in the New Jersey Local Redevelopment and Housing Law (LRHL) at N.J. S.A. 40A:12A-14. The LRHL allows a municipality to prepare a redevelopment plan which provides the development regulations and other standards to guide future development for this area.



LANGAN
NORTH JERSEY MEDIA GROUP
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SURROUNDING AREA CONTEXT:

The five parcels (Block 203.01 – Lots 2, 3 and 3.01, Block 300 – Lots 3.01 & 3.02) are located between Bridge and River Streets within the Downtown Rehabilitation Area Zoning District which was adopted in June 2012. The current land use for the properties include surface parking lots, and commercial establishments ranging from one story to two story buildings.

PROPERTY INFORMATION:

Block 300 - Lot 3.01: The largest of the five (5) lots, this property is approximately 17.9 acres or 779,724 sq. ft. and includes several buildings formerly occupied by The Record Newspaper. The property includes the main building which has three separate sections including: A three story masonry, metal and glass building which fronts on River Street; A four story masonry building in the center and; A one story masonry building at the rear of the property. In addition to the main office building there are two buildings one of which is a masonry and metal building and the other is a concrete block and metal building.

All of the buildings were designed for the purpose of newspaper publishing and distribution. The facility was vacated in 2008. All utilities have been disconnected and the sprinkler system has been deactivated. In general the buildings have fallen into a state of disrepair as to be untenable with a significant amount of water damage, mold, and deterioration throughout all three buildings. The width and size of the main building, the significant high ceilings to accommodate the previous printing presses, and the differing finished floor elevations in the different sections would indicate there is no other practical reuse for the buildings which would render them obsolete. The description and photographs indicate the amount of disrepair, and dilapidated state of the buildings.

The inside of this building has been declared unsafe building by the Building Department. The main building has signs of significant water damage for each floor. In the floors with offices, a significant amount of damage can be seen in the walls, floor and ceilings.

The one story warehouse building at the northeastern side of the lot has one singular garage door in the rear, with one door and an overhang at the front and two doors on the side. The inside of this building had water puddling on the floor, with debris throughout.

The one story warehouse building to the south of the first warehouse, is separated by a small parking lot containing 45 parking spots for cars and 13 parking spots for delivery trucks. The outside of the warehouse has several garage entrances for delivery trucks. The inside of the building contains both standing water with signs of previous



Hackensack Rehabilitation Area
The Record Site Redevelopment Area



Bird's Eye View
The Record Site Redevelopment Area

standing water. The bathroom has standing water with molding and damage to the walls, floor and ceiling.

In addition to the three buildings used for newspaper publishing and distribution, the property includes the USS Ling and a separate one story trailer museum. The submarine is docked along the Hackensack River. The parking lot closest to the museum had standing water. The various buildings appear to be temporary structures that are insignificant.

Furthermore, during Superstorm Sandy the property was inundated with water reaching River Street which caused considerable damage and would likely be a contributing factor toward the existing mold located in the structures.

Block 300 - Lot 3.02: The building located on 80 River Street is a one story masonry building that is approximately 4,092 sq. ft. The lot is approximately 200' wide x 190' deep. The building has three (3) entrance ways that are elevated. The front entrance is an ADA accessible entrance way for the public. The building is currently occupied by the Heritage Diner restaurant. This Block and Lot like the others are owned by Bergen Evening Record Corp. / Macromedia Incorporated.

Block 203.01 - Lot 3.01: The Block and Lot is an open parking area located on 62 River Street and is the smallest of the parcels in the Investigation Area have a lot area of 6,130 sf. This lot is a part of the parking area with Lots 2 and 3 for Block 203.02 Lots 2 and 3. This Block and Lot like the others are owned by Bergen Evening Record Corp. / Macromedia Incorporated.

Block 203.01 - Lots 2 & 3: These Block and Lots are grouped together according to the City tax records. The lots are irregularly shaped with an area of 19,400 sf. These lots along with Block 203.01 Lot 1 are comprised of 62 parking spaces. This Block and Lot like the others are owned by Bergen Evening Record Corp. / Macromedia Incorporated.



Hackensack Rehabilitation Area
Regional Context Map

REQUIRED COMPONENTS OF THE REDEVELOPMENT PLAN:

N.J.S.A 40A:12A-7 requires that a redevelopment plan include an outline for the planning, development, redevelopment, or rehabilitation of the Redevelopment Area sufficient to indicate the following:

1. Its relationship to definite local objectives as to appropriate land use, density of population and improved traffic and public transportation, public utilities, recreational and community facilities and other public improvements.
2. Proposed land uses and building requirements in the project area.
3. Adequate provisions for the temporary and permanent relocation, as necessary for residents in the project area, including an estimate of the extent of which decent, safe, and sanitary dwelling units affordable to displace residents will be available to them in the existing local housing market.
4. An identification of any property with the redevelopment area which is proposed to be acquired in accordance with the redevelopment plan.
5. Any significant relationship of the redevelopment plan to:
 - The master plans of contiguous municipalities;
 - The master plan of the county in which the municipality is located;
 - The State Development and Redevelopment Plan adopted pursuant to the “State Planning Act” P.L. 1985,c.398 (C.52:18A-196 et al.).
6. An inventory (as of the date of the adoption of the resolution finding the area to be in need of redevelopment) of all housing units affordable to low and moderate income households, as defined pursuant to N.J.S.A. 52:27D-304, that are to be removed as a result of implementation of the redevelopment plan, whether as a result of subsidies or market conditions, listed by affordability level, number of bedrooms, and tenure.
7. A plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan .
8. The redevelopment plan may include the provision of affordable housing in accordance with the “Fair Housing Act,” N.J.S.A. 52:27D-301 et seq. and the housing element of the municipal master plan.
9. The redevelopment plan shall describe its relationship to pertinent municipal development regulations as defined in the “Municipal Land Use Law,” P.L.1975, c. 291 (C.40:55D-1 et seq.).
10. The redevelopment plan must state whether it shall supersede applicable provisions of the development regulations of the municipality or constitute an overlay zoning district within the redevelopment area.
11. All provisions of the redevelopment plan shall be either substantially consistent with the municipal master plan or designed to effectuate the master plan; but the municipal governing body may adopt a redevelopment plan which is inconsistent with or not designed to effectuate the master plan by affirmative vote of a majority of its full authorized membership with the reasons for so acting set forth in the redevelopment plan.

DEFINITIONS:

It is the intention of this Redevelopment Plan for the Main Street Rehabilitation Area to supersede existing zoning (except as noted herein) as provided under the Local Redevelopment and Housing Law, N.J.S.A 40A:12A-1, et seq. However, this plan adopts the definitions of the City's Land Use Ordinance Section 175-2.2 by reference.

The definitions of the City's Land Use Ordinance shall apply to this plan, unless this plan provides a superseding definition. The definitions defined in Section 175-2.2 apply to the redevelopment area with the exception of the following terms:

FACADE: Any exterior building face or wall.

STREET FURNITURE: The elements of streetscape, including but not limited to benches, bollards, news-racks, trash receptacles, tree grates, hardscape, seat-walls, street lights, and street trees.

BUILDING PROPORTION: The width to height relationship of one building to another.

PEDESTRIAN SCALE: The relationship of a particular building, in terms of mass and scale to a pedestrian.

VISUAL COMPATIBILITY: The design of buildings which is consistent in scale and character with adjacent buildings in the area from a pedestrian perspective in which the design is consistent with the architectural and neighborhood design standards set forth in this document.

BUILDING BASE: The building base refers to the first three levels of a particular building:

BUILDING HEIGHT: The vertical distance of a building measured from the average of the top of curb of the two corners of the proposed building to the highest roof beams of a flat roof. For projects located at a corner lot, the height will be measured from the top of curb of the two corners located along the primary building facade.

BUILDING SETBACK LINE: The distance measured from the back of existing or proposed curb (whichever is greater) to the primary building facade.

POP-OUT / PROTRUSION: A portion of the primary building facade that projects into the building setback.

REDEVELOPER: Any person, firm, corporation or public entity that shall voluntarily seek and be designated as a Redeveloper by the City Council or Redevelopment Entity and shall enter into a Redevelopment Agreement as set forth in Section 5 of this Rehabilitation Plan, all in accordance with the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-1 et seq.

RESTAURANT: A building or structure designed, used or intended for use in which food and beverages are sold and consumed.

TANDEM PARKING: Two parking spaces with one car in front of the other to be allocated to a single apartment.

EXISTING ZONING:

The property is located in the B-3 District and include the following requirements:

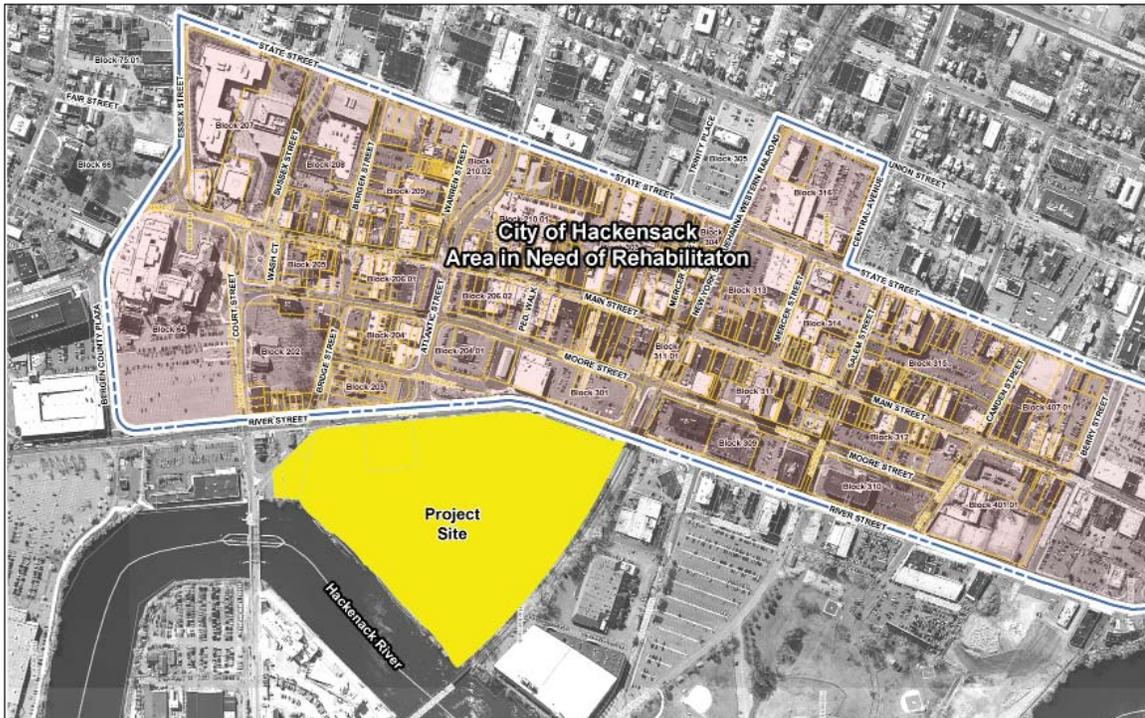
Permitted Principal Uses:

No building or premises shall be erected, altered or used except for uses designated for each district or use as follows:

- a) All uses permitted in the B-2 District except multi-family dwellings
- b) Newspaper office
- c) Publishing and job printing, including blueprints, photostats, offsets and other similar reproductions.

Area, Yard and Bulk Regulations:

	Hotel	Non-Res
a. Min. Lot Size (sq. ft.)	1.5 Acres	0
b. Min. Lot Width(ft.)	100	0
c. Min. Front Yard (ft.)	20	15
d. Min. Side Yard, interior (ft.)	15	10
e. Min. Side Yard, secondary streets (ft.)	15	10
f. Min. Rear Yard (ft.)	15	10
g. Max. Height (stories/ft.)	0/ 150	0/150
h. Max. Lot Coverage (%)	33 1/3	25
i. Min. Lot Area per Dwelling Unit (sq. ft.)	0	0
k. Min. Height Ratio, front	4:1	6:1
l. Min. Height Ratio, side	4:1	4:1
m. Min. Height Ratio, rear	4:1	6:1



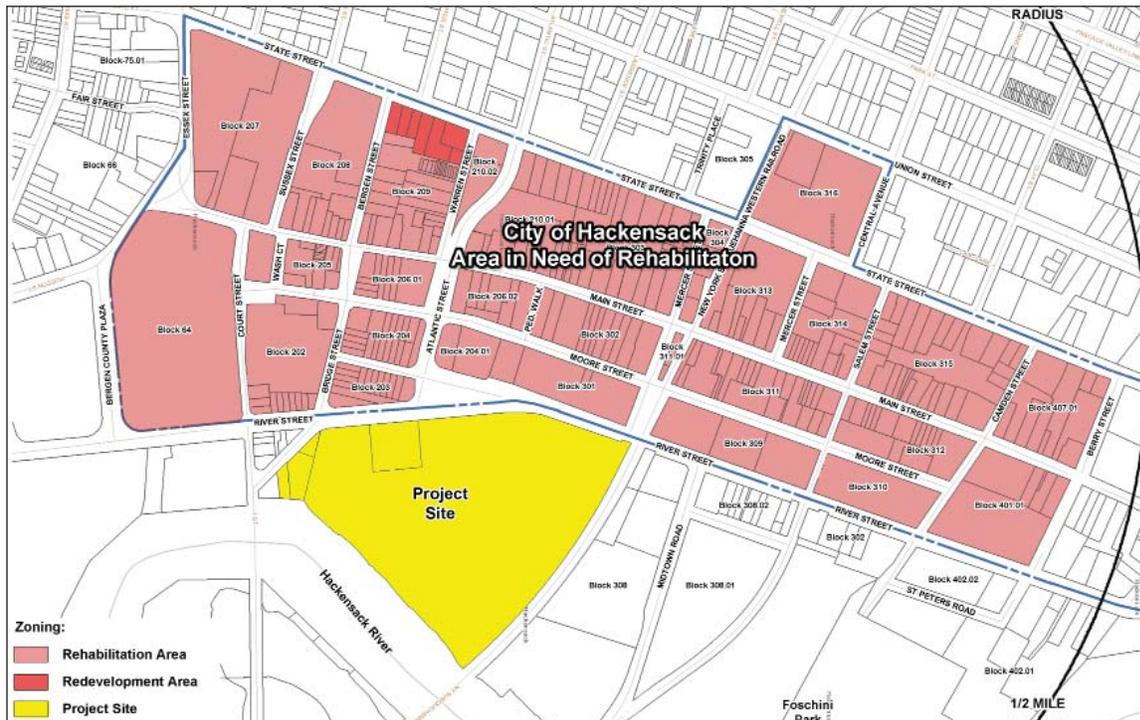
Hackensack Redevelopment Area
Aerial Map

B-2 District Permitted Uses:

- All uses permitted in the B-1 District except studios for instruction of self-defense, day nurseries, nursery schools, and townhouses;
- Appliance stores;
- Book and stationary stores;
- Business or vocational schools;
- Department stores;
- Dry goods and variety stores;
- Fully enclosed theaters, concert halls, auditoriums;
- Furniture stores;
- Hardware and for building supply stores;
- Hobby and craft stores;
- Movie theaters, bowling alleys, and other indoor amusement facilities;
- Office equipment establishments;
- Painting, plumbing and wallpaper stores;
- Photographic equipment and supply stores;
- Supermarkets;
- Telegraphic office, telephone exchange

B-1 District Permitted Uses:

- Retail stores and shops
- Art gallery, studios for dancing, photography, sculpture or musical instruction or instruction in physical self defense;
- Bakery where products prepared are retailed on the premises;
- Bank or trust company or savings and loan institutions, except drive-in facilities;
- Club, lodge, meeting hall and social recreation building affiliated with a national or international organization;
- Delicatessen store;
- Drug stores;
- Florist shop;
- Funeral parlors, undertaking establishments;
- Hardware stores;
- Municipal, county, state or federal governmental building, library, park or recreation facility, firehouse;
- Package liquor store;
- Personal service establishments, including tailor, dressmakers, shoe repair, barbershop, beauty parlors, nail and hair salons, excluding public garages;
- Pet shops;
- Professional offices, business offices, governmental offices, office buildings;
- Travel agencies or offices.



Hackensack Redevelopment Area Land Use Map

REDEVELOPMENT PLAN GOALS:

This property is identified as a potential redevelopment area in both the 2001 Master Plan and the 2009 Re-examination Report and as a part of the Downtown Rehabilitation Planning process the city and its residents developed goals and objectives for the revitalization of the downtown into a vibrant mixed use pedestrian friendly environment. The following is a list of the goals which relate to the redevelopment area:

1. To promote and strengthen existing businesses with the creation of a livable, real and clearly defined downtown district, made up of a series of interconnected, newly formed, mixed-use neighborhoods which are connected to their immediate context;
2. To promote the development of a place-based environment where people live and work connected by great streets and activated by appropriate street retail, food and entertainment uses;
3. To promote a balance between pedestrian and vehicular importance through the design of a hierarchy of streets, roads, gateways and boundaries which promotes activity and vitality;
4. To promote a series of memorable public places and spaces connected by great sidewalks and streetscape elements, enhanced by public art, safe lighting and on-street parallel parking, that promote outdoor dining, walking, people watching and gathering, activated by a clearly organized program of street retail and restaurants;
5. To promote improved pedestrian and vehicular connectivity into and through the downtown and to adjacent existing residential and commercial neighborhoods as well as to transportation and transit options;
6. To promote an overall approach to parking based on a shared vertical strategy reflective of a mixed use environment which emphasizes quality, accessibility, location, size, scale, configuration, management and aesthetic character over quantity;
7. To promote and take advantage of the physical relationship of the downtown to the County Complex, Bergen Community College, Fairleigh Dickinson University and Hackensack University Medical Center;
8. To promote the development of a compact urban environment with improved connectivity to the existing rail and bus transit in order to encourage walking and minimize vehicular dependency;
9. To promote and improve the efficiency and capacity of the existing street network to better accommodate vehicles, pedestrians and bicycles in the context of a complete street;
10. To promote a high density, mixed use development with a diverse residential population of varied ages, races and socio-economic backgrounds;
11. To promote the principles of sustainable design for individual buildings and for the district as a whole.

REDEVELOPMENT PLAN OBJECTIVES:

The City is looking to promote a mixed use predominantly residential development that would support the continuing revitalization efforts in the downtown as a part of the 2001 Master Plan, 2009 Re-examination Report and the 2012 Downtown Rehabilitation Plan.

The objectives for this Redevelopment Plan are to:

1. Promote increased quality of life for all residents in the City of Hackensack;
2. Promote the development of a mid to high density predominantly residential mixed use project;
3. Support existing businesses and property owners while creating new construction and permanent jobs in the City;
4. Generate new tax ratables and revenue from the sale of the property to support additional revitalization activities;
5. Implementation architectural, neighborhood and sustainable design standards that promote high quality development.
6. Provide parking to support the residential and retail uses in the redevelopment area.

The redevelopment plan is intended to capitalize on its prime location and proximity to the Downtown Rehabilitation Plan, the New Jersey Regional Bus Station, the two New Jersey Transit Stations (Essex and Anderson Street), as well as to the major transportation including Route 4, 46, 17, Interstate 80 and the Garden State Parkway.



USE AND BULK REQUIREMENTS:

PERMITTED LAND USES:

The following uses are permitted in the Redevelopment Plan:

RESIDENTIAL:

Multi-family, apartments, condominiums, townhomes, residence halls, lofts, and live work studios;

RETAIL:

Retail stores and shops, restaurants, eating and drinking establishments, bakery, delicatessen, movie theater, grocery store, hardware, book and stationery, florists; free standing or as liner uses around parking structures;

HOTEL: Minimum 100 rooms

COMMERCIAL:

General office, medical, child care, physical therapy, professional uses, banks, pharmacy, health clubs, art galleries and studios, museum, child care; as a liner around parking structures;

PUBLIC / CIVIC:

Parks, plazas, open space, riverfront amenities, civic, museum, public facilities;

ADDITIONAL REQUIREMENTS:

1. Any use not specifically stated as a Permitted Use is not allowed in the Redevelopment Area;
2. Drive-thrus are permitted for Retail and Commercial uses pursuant to the requirements identified in the Design Standards of this Redevelopment Plan;
3. Parking structures shall be screened from River Street, internal streets and the Hackensack River with uses or based on the architectural requirements identified in the Design Standards of this Redevelopment Plan;
4. All parking shall be screened from any public street per the Design Standards of this Redevelopment Plan;
5. A minimum 80'-0" Bus Stop shall be provided on the east side of River Street across from the existing NJ Transit Regional Bus Station.
6. A prominent connection shall be provided to the Hackensack Riverfront walkway from River Street along the Atlantic Street extension.
 - a. Retail uses are strongly encouraged along this access and to the river walk.
 - b. At the intersection of River Street and the Atlantic Street extension, a minimum 18'-0" building setback from the proposed curb is required. Parking along this section is prohibited up to the first intersection.
 - c. For the development adjacent to and fronting on the Hackensack River and beyond the first intersection of the Atlantic Street extension, an approximately 164'-0" dimension between buildings is encouraged to permit adequate space for public parking, outdoor dining, public events, and open space that will provide a visual connection and public access to the Hackensack River.

ACCESSORY LAND USES:

1. Lobbies on the ground floor providing access to residential uses on the upper floors;
2. Parking as either structured or surface as an accessory use to a permitted use;
3. Loading spaces and docks, recycling and refuse storage areas;
4. Retail, commercial, office and service uses provided they are subordinate to the principle permitted use including:
 - a. Management and Leasing Offices;
 - b. Conference center, meeting rooms;
 - c. Fitness Centers;
 - d. Outdoor pool and amenity facilities
 - d. Walk up ATM's;
 - e. Spa, beauty and coffee shops;
 - f. Dry cleaning services;
5. Accessory structures shall comply in all respects with the requirements of the principal structure;
 - a. No accessory structure shall be located closer to the street right-of-way line than the required front yard setback of the principal use;
 - b. No portion of an accessory structure shall include living quarters except parking structures as an accessory use may contain residential over the structure;
 - c. The square footage of an accessory use shall not count toward the maximum square footage for retail, commercial or civic uses;
6. When an accessory structure is attached to the principal structure, it shall comply in all respects with the requirements applicable to the principal structure;
7. No accessory structure shall be constructed or placed on any lot unless the principal structure is first constructed or placed upon said lot, with the exception of parking facilities (surface or structure) which may be constructed prior to the primary use;
8. In no event shall the height of an accessory structure exceed the height of the principal building with the potential exception for parking structures which will be allowed to exceed the primary structure for access only;



Retail Street Section adjacent to the Hackensack River

DENSITY, AREA, YARD AND HEIGHT REQUIREMENTS:

The following requirements apply to the Redevelopment Area:

1. **Minimum Lot Size:** None
2. **Minimum Dwelling Unit Size:** 500 sf
 - a. A maximum of 25% of the units can be between 500 sf and 650 sf for each proposed phase;
 - b. A maximum of 25% of the units can be over 1,200 sf for each proposed phase;
 - c. Each phase shall comply with the minimum / maximum unit size percentage requirements;
3. **Minimum / Maximum Building Height:** 24'-0" / 92'-0"
 - a. All freestanding commercial / retail / office uses shall have a minimum building height of 24'-0".
 - b. All other buildings are permitted up to 92'-0".
 - In addition, 25% of the footprint of all residential buildings in the Redevelopment Area shall be permitted for up to 176'-0" (excluding roof top equipment).
 - c. Building heights do not include roof top equipment or parking structure access;
 - Maximum height of appurtenances shall be 15'-0" (See screening requirements in the Design Standards section of the Redevelopment Plan);
 - Maximum roof coverage by appurtenances is 25%;
 - d. Corner elements that extend above the main facade are strongly encouraged and may extend beyond the maximum building height;
 - e. All structures shall be designed with a flat roof;
 - f. The first floor of any non-free standing commercial, retail or hotel use shall have a minimum floor to floor height of 16'-0";
5. **Maximum Number of Units:** 700 Units
6. **Maximum Retail / Commercial Space:** 70,000 sf
 - a. The minimum depth for attached retail or commercial space shall be 40'-0";
7. **Maximum Civic Space:** 10,000 sf
8. **Hotel:** Minimum 100 rooms
 - a. A minimum 1.35 ac site shall be set aside for a hotel for a minimum 4 years after the issuance of the first residential building permit for vertical construction in Phase 1.
9. **Minimum / Maximum Building Setbacks:** All building setbacks are measured from the existing or proposed face of curb (which ever is greater).
 - a. River Street / Bridge Street:
 - Minimum building setback 22'-0"
 - Maximum building setback 30'-0"
 - b. Atlantic Street Extension / Streets with Retail Frontage
 - Minimum building setback 18'-0"
 - Maximum building setback 30'-0"
 - c. Internal Streets:
 - Minimum building setback 12'-0"
 - Maximum building setback 18'-0"

- d. No internal front, side or rear yard setbacks are required between existing or future property lines;
 - e. Utility structures, exhaust air vents, backflow preventers, or other similar devices when located above grade, must be located behind the setback, be screened and cannot be located on either River Street;
 - f. Utility structures located below grade may be located within the setback of any street;
10. **Building Setback along the Hackensack River:**
- a. A minimum 30'-0" riverfront public access walkway shall be provided along the Hackensack River (See Design Standards for requirements);
 - An additional 21,920 sf of public parks, public open space and / or public plazas shall be provided as a part of the development.
 - Sidewalk areas, streetscape and / or courtyards elements do count toward this requirement.
11. **Maximum Building / Lot Coverage: 80%**
12. **Phasing:**
- a. The maximum number of phases shall be four (4).
 - The development of a hotel is not included in the number of phases.
 - b. Each phase will be required to submit for site plan approval pursuant the requirements of the Redevelopment Plan;
 - c. Residential is required as a part of Phase 1.
 - d. All parks and open space improvements including but not limited to public parks, open space and the Hackensack River Walk shall be implemented in the first phase of development;



Artist Perspective of Retail Street adjacent to the Hackensack River

PARKING / ROADWAY STANDARDS:

Parking Requirements for the redevelopment area include:

Use	Parking Ratio
Residential:	1.5 sp per dwelling unit + 10% for guest parking
Retail/Commercial:	3.5 sp/1,000 GFA*
Hotel:	0.75 sp per room + 1 sp per employee
Civic / Public:	1.0 sp per every 4 seats

Notes:

1. When the formula or parking spaces required results in a fraction of a space exceeding 0.49, a full space shall be required;
2. Gross Floor Area is the total interior floor area of all floors determined by measuring the inside dimension of the outside walls of the structure;
3. Up to 10% of the required parking stalls may be designated for compact cars;
4. Tandem spaces are prohibited;
5. Outdoor seating areas do not count toward the required parking ratios;
6. Accessory uses do not require parking;
7. The minimum parking setback along River Street shall be 22'-0";
8. All surface parking shall be screened based on the Design Standards indicated in the Streetscape section of this Redevelopment Plan;
9. For Civic Uses without seating, the Retail/Commercial parking requirements shall apply;

Public Roadways:

1. All internal roadways shall meet the standards set forth in the City of Hackensack Zoning Ordinance with the following provisions;
 - a. Roadway Dimensions:
 - The minimum roadway width shall be 24'-0" (12'-0" travel lanes) excluding required turning lanes at intersections into the Redevelopment Area;
 - The minimum parallel parking width shall be 8'-0".
 - The minimum head-in parking width shall be 18'-0"
 - b. Sidewalk Width: The following sidewalk and landscape median widths are required;
 - The minimum sidewalk width shall be 6'-0" for all internal streets.
 - The minimum sidewalk width along the Hackensack River Walk shall be 6'-0";
 - A minimum 12'-0" landscape area is required along River Street between the existing or proposed curb and the sidewalk with the exception of a bus pull-out on River Street;

Shared Parking:

Joint use of up to 50% of the required retail, commercial and hotel parking spaces only may be permitted for two or more uses located within the redevelopment area, provided the development can demonstrate the uses will not substantially overlap in hours of operation, or in demand for the shared spaces. Retail, commercial and hotel spaces are required to be located in an area that is

available to the public. On-street parking is permitted to count toward the required retail parking. The shared parking analysis must demonstrate the shared parking plan is feasible based upon a “Shared Parking Analysis” using Urban Land Institute’s (ULI) Shared Parking Software or a comparable software model. The Shared Parking Analysis shall be prepared by a credible expert, such as an experienced parking or land use consultant, planner, architect or engineer.

Setback Maneuvering:

No surface parking or maneuvering space is permitted in any required setback, or between the permitted use and the required setback, except driveways providing ingress and egress to the parking or service area may be installed across these areas.

Bicycle Parking:

Bicycle parking is required in publically accessible areas for new developments at one bicycle parking space for every 25 vehicle parking spaces, up to 200 vehicle parking spaces.

- a. Thereafter, one space shall be provided for every 100 vehicle parking spaces. Fractions equal to or greater than one half shall be considered to be one bicycle space;

Handicapped Accessible Parking Requirements:

Refer to Section 175-10.1C Handicapped Parking Schedule (City of Hackensack Zoning Ordinance).
Additional Off-Street Parking Regulations:

For all parking standards not covered in this section refer to Section 175-10 Off Street Parking Regulations with the exception of the following:

- a. Off-street parking dimensions shall be a minimum of 9’-0” x 18’-0” (Dimensions do not include columns in structured parking);
- b. Two lane drive aisle dimensions within a parking structure can be 23’-0”;



Multi-family residential building typ.



Central landscape median with retail shops lining the street.

DEVELOPMENT / PLACE-MAKING DESIGN STANDARDS

The design standards in this section provide the criteria for the redevelopment area. Any future development is subject to these provisions and shall be built in accordance with the minimum design standards specified in this section.

Building Architectural Materials & Character:

1. All buildings shall be constructed with high quality materials that reinforce pedestrian scale;
2. Primary building materials shall include: brick, stone, and/or glass which cover a minimum of 65% of each building façade, exclusive of windows and doors with accent materials comprising a maximum of 35% for each building façade;
 - Vinyl siding of any type or grade is strictly prohibited on any portion of building façade in the redevelopment area;
3. Elements such as cornices, belt courses, corbelling, molding, string courses, ornamentation, changes in material or color, and other sculpturing of the base are appropriate and should be provided to add special interest;
4. Special attention must be given to the design of windows at the base of buildings. Ribbon windows are strongly discouraged. Recessed windows that are distinguished from the shaft of the building through the use of arches, pediments, mullions, and other treatments are encouraged;
5. Buildings shall be broken up vertically into a base, a middle and a top and horizontally into bays, through the use of building articulation or change in materials. The base should not extend beyond the first three levels with an emphasis on providing design elements which enhance the pedestrian environment particularly at the street level;

Concept Elevation



Conceptual Building Design with unique first floor design to provide variety at the streetscape



Typical Streetscape with Retail on the first level

- Material changes should occur at logical points delineated by a change in the building or cornice. For buildings in excess of 120'-0" in length a vertical demarcation should be included at a minimum of every 60'-0" of building façade;
- 6. Any retail, commercial or office storefront design at the first level should reflect the individual tenant's brand identity;
- 7. Generic national branding architecture for freestanding retail / commercial buildings unless it meets the requirements of this section is prohibited;
 - All freestanding buildings shall be constructed with the similar building materials as the residential/mixed use buildings so that all buildings in the development have the same or a similar architectural character;

Building Orientation:

1. Buildings shall be oriented toward public streets with the primary entrances for retail and residential;
2. The first floors of all buildings, including structured parking, must be designed to encourage and complement pedestrian-scale and activity;



Artist rendering of Hackensack Riverfront walkway



Outdoor dining within the public right of way



Public open space and landscape amenity



Streetscape with retail

3. Drive-thru's are permitted in the Redevelopment Area so long as they are not located between the building and River Street and they are screened from view from any public street with either a building use or a masonry wall.

Building Entrances:

1. Building entrances should be easily identifiable with unique and interesting signage for each individual use;
2. Retail entrances can have up to a 4'-0" pop-out / protrusion / recess into the setback or adjacent public open space for columns or other architectural features that distinguish the storefront;
3. Entrances for residential uses should be separate and distinct from retail uses;

Street Level Frontage / Uses:

1. Retail and commercial uses are strongly encouraged to front onto the public park at the Atlantic Street extension and the Hackensack River;
2. Outdoor dining is encouraged and should meet the City of Hackensack Building Department requirements;

Structured Parking:

1. Structured parking shall be screened from River Street, and the Hackensack River;
2. Parking structures shall be screened from River Street, the Hackensack River and internal streets with either uses or shall include decorative architectural details such as building materials, windows and detailing;
 - These elements should have the same materials and a similar design and scale of openings as the residential above;
 - Other than at the parking entrances vehicles shall not be visible from the center line of either public street.
3. All ramps shall be internal to the parking structure and shall not be visible from any public street;
 - Cabling or exposed concrete alone does meet the screening requirement;
4. Driveway and garage openings should not exceed 28'-0" in width and should include traffic calming measures and a change in surface materials where driveways cross the sidewalk to help ensure pedestrian and bicycle safety;



Structured parking facade screening



Canopies and balconies for residential



Architectural interest at building corners

Surface Parking Screening Requirements:

1. All surface parking shall be screened from any public street with a minimum 4'-0" masonry wall (or materials that match the architecture of the adjacent buildings) and masonry cap. All walls shall be designed and constructed with the same materials.
2. All masonry walls shall include a pillar that extends a minimum 4" beyond and 4" above the wall. Pillars shall be located at minimum every 50'-0" or at any change in direction.
3. In locations where parking or drive access is provided the masonry wall shall turn and extend a minimum 8'-0" and shall terminate with a pillar.
4. A minimum 3'-0" landscape area shall be located between the wall and the sidewalk.

Canopies and Balconies:

1. Vertical metal canopies, awnings, and similar architectural accents are encouraged at entrances to buildings. Such features should be constructed of rigid materials designed to complement the tenant's identity at the street level.
2. Any canopy may extend from the building up to one half of the width of the setback area in front of the building, or eight (8) feet, whichever is less;
3. Ground supports are not permitted in the minimum setback, sidewalk or in the public right-of-way;
 - These features should not extend over or interfere with the growth or maintenance of any required tree plantings;
4. The minimum overhead clearance shall be ten (10'-0") feet. If a canopy, awning, cornice, or other appurtenance extends into the public right-of-way, an encroachment agreement shall be required;
5. Balconies/balconettes can be incorporated into the building façade and are allowed to project into the building setback up to 6'-0" starting at the second level;

Mechanical Equipment Screening:

1. Screening of rooftop mechanical equipment is required.
2. All rooftop mechanical equipment including cell phone antennae shall be screened from view from all adjacent public streets, open spaces and parks in all directions and elevations to minimize the negative impact;

3. Screening materials shall be the same as the architectural detail, color and materials of the building; Wire mesh is not permitted;
4. All roof and HVAC systems must meet the building code requirements and at minimum be set back from the building edge a minimum of 15' from any street or public open space and screened as to not be visible from any adjacent public street or public property;
5. If wall pack ventilation units are being used they are required to match the adjacent building material color;

Building Service Locations:

1. All service and delivery locations for the redevelopment area shall be provided in a location that is not visible from River Street;
2. All service and delivery maneuvering into the loading area shall occur entirely within the property line other than for ingress and egress to the lot.

Doors and Windows:

1. Where expanses of solid wall are necessary, they may not exceed 30'-0" feet in length without architectural enhancements including display windows;
2. The first floor and street level shall be designed to address all public streets;;
3. For buildings fronting two streets or one street and a public open space the entrance should provide access from the primary street and is encouraged to provide access from the secondary street or public open space;
4. For buildings that do not provide access from the secondary street or open space, the building facade should provide windows and/or architectural details that are aesthetically interesting;
5. For retail uses operable windows are encouraged;
6. No development subject to these provisions may have exterior walls with a reflectivity value in excess of 35 percent;

Openings / Ventilation:

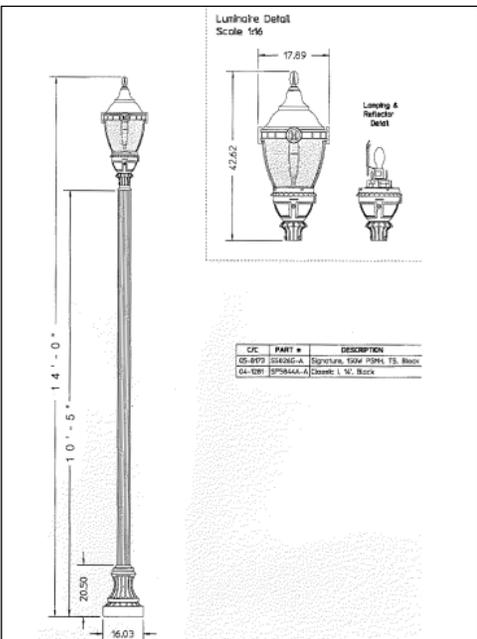
1. Any openings for ventilation, service, or emergency access located at the first floor level must be decorative and must be an integral part of the overall building design;
2. Openings as well as pedestrian and vehicular entrances must be designed so that cars parked inside are not visible from the street;



Streetscape with ground floor retail



Streetscape with pedestrian scaled lighting and crosswalks



Hackensack Typical Street Light Detail

STREETSCAPE / PARKS AND OPEN SPACE DESIGN STANDARDS:

A successful neighborhood is not complete until its parks, open space, sidewalks and streetscape have been designed and “furnished”. The items that are referred to as streetscape elements include street lights, sidewalks, benches, trash receptacles and other street furniture which reinforce the character of the street and the neighborhoods. The following section addresses an overall approach for the design of streetscape elements that are to be considered as part of the redevelopment plan. These standards are meant to establish the minimum criteria that will be required for the design and implementation of streetscape, parks and open space improvements.

Street Trees:

1. Street trees shall be planted in either grates or open landscape areas equivalent to 30’ on center along all public street frontage;
2. Street trees shall be planted with a minimum four inch (4”) caliper, shall be trimmed up to 8’-0” and shall be in accordance with the “American Standard for Nursery Stock” published by the American Association of Nurserymen;
3. Street tree species shall be coordinated with the City of Hackensack shall be installed along public streets.

Street Lighting:

1. Street lights shall be the City of Hackensack city standard pedestrian scale light pole and fixture;
2. Street lights shall be shielded from second floor uses and shall be a maximum of 16’ in height, located on center between street trees, at a maximum of 90’-0” on center (Photo-metrics shall be provided to determine the final street light spacing);
3. Street light specifications and locations shall be submitted for review and approval prior to installation;
4. Street lights should be located as part of the streetscape and function as a unifying element of other streetscape items including trees, benches and paving;
5. Building light fixtures should exhibit an aesthetic as well as functional purpose to create interest and a sense of scale for the pedestrian;
6. LED lighting is encouraged;
7. Luminaires should be translucent or glare-free using opaque glass or acrylic lenses;

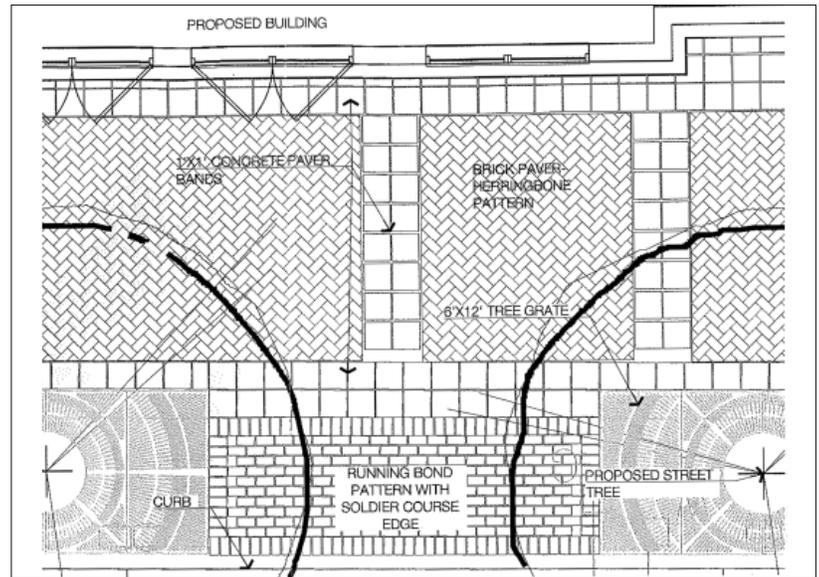
- Diffusers and refractors should be installed to reduce unacceptable glare; particularly adjacent to residential uses;

Parks, Open Space, Riverfront Walk:

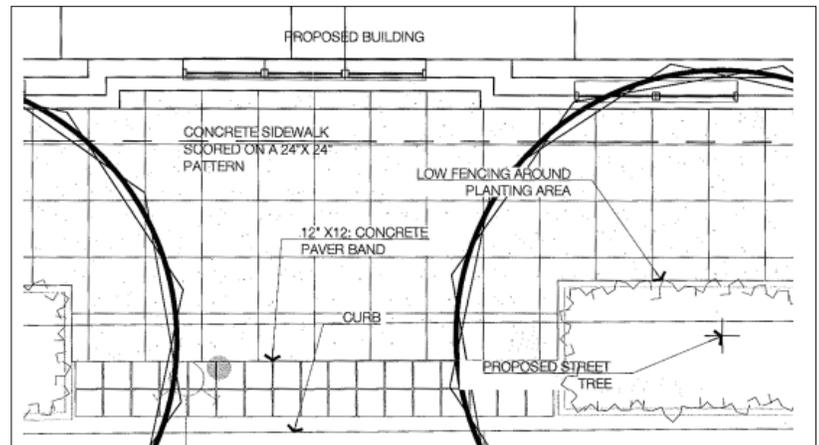
- A minimum 50' easement shall be provided along the Hackensack River which shall include an 6'-0" sidewalk with landscape trees, lighting, benches and trash receptacles located at the minimum distance requirements for each element;
- At minimum a public plaza shall be located at the terminus of the Atlantic Street extension and shall include enhanced hardscape materials, lighting, seating, as well as a public area for outdoor gatherings and events;

Sidewalks:

- Sidewalks should be a minimum of 6'-0" and may consist of brick, concrete or a combination of the two and should be consistent for the all public streets in the redevelopment area;
- Streets with proposed retail uses and outdoor dining on the first level are strongly encouraged to install the Typical Streetscape Pattern for Retail Streets;
- All other streets are encouraged to install the Typical Streetscape Pattern for Secondary Streets;
- Asphalt of any type or grade is not permitted as a sidewalk material;



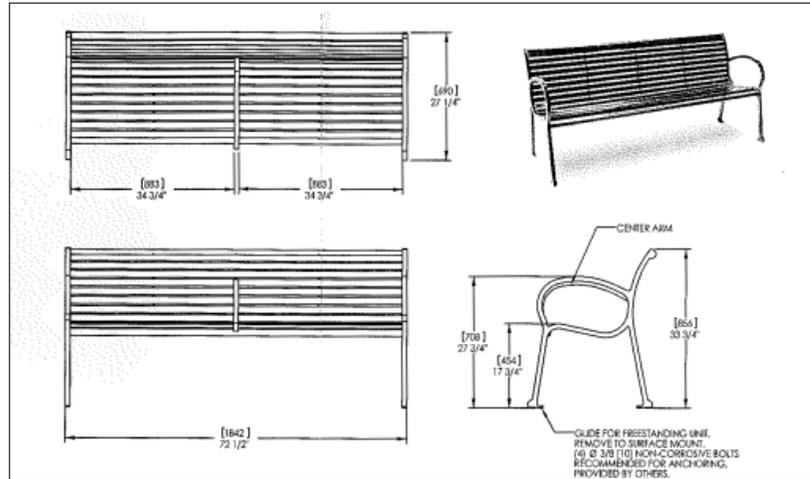
Typical Streetscape Pattern for Streets with Retail Frontage



Typical Streetscape Pattern for Streets with Retail Frontage

Seating, Benches:

1. Seating should be grouped together as much as possible and placed in public open spaces, plazas or gathering places;
2. In areas with high pedestrian volumes and generally along retail frontages should include two benches and one trash receptacle approximately every 90' to 120' on center.
3. In areas with less intensive pedestrian volumes two benches and one trash receptacle should be placed approximately every 180' to 240' on center.
4. Seating should not obstruct views for vehicles, sidewalks for pedestrians, or signs and displays for businesses;



Typical Streetscape Pattern for Streets with Retail Frontage

Planting Pots & Planters:

1. Pots and planters can be used particularly in areas with retail as a complement to the surrounding streetscape and are permitted in the setback with review and approval of the Building Department;
2. Pots and planters can be placed as a separation between pedestrians and vehicles;
3. Pots and planters are ideally located near seating areas, but plant materials should not interfere with circulation or comfort;

Tree Grates:

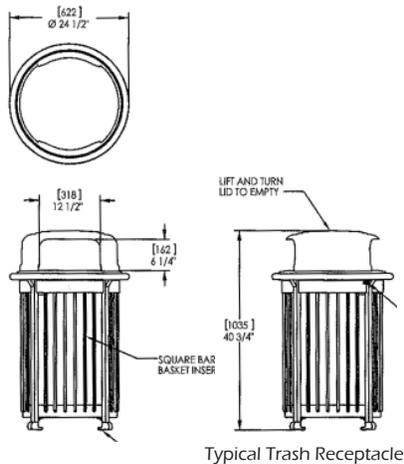
1. Tree grates are permitted and can be used wherever a tree is placed within a high traffic area;
2. Tree grates must allow for tree growth and be made of ductile iron and should be factory painted;
3. Electrical outlets should be provided within the grate area to allow for lighting opportunities and should be designed to support up-lighting;

Trash Receptacles:

1. Trash receptacles should be located conveniently for pedestrian use and service parks, plazas and other public gathering places.
2. Trash receptacles should be permanently attached to deter vandalism and have sealed bottoms with sufficient tops to keep contents dry and out of pedestrian view;

Bicycle Racks:

1. Bicycle racks should be permanently mounted and placed in convenient public locations to encourage bicycle use;
2. Bicycle racks should be placed so as not to obstruct views or cause hazards to pedestrians or drivers;
3. At least one bicycle rack shall be provided on each block on all streets;



Bicycle rack



Bollards with lighting

Bollards:

1. Bollards should integrate with and aesthetically complement the overall streetscape concept;
2. Bollard design should respond to the area it supports (Plaza vs Services);
3. Bollards may be chained or cabled together to ensure pedestrian safety or define areas for public functions;

Public Art:

1. Public art should be “accessible”; tie to the history of Hackensack and if possible, should be created by a local source;
2. Public art may include water, seating, planting, decorative architectural elements or plaza space as part of the design;
3. Public art should be visible, but not interfere with pedestrian circulation or create a traffic hazard and should be made of durable, weatherproof materials and should be crafted, not “sharp”;

Parking Meters:

1. On-street parking meters are required along any existing and new public street in the redevelopment area;
2. Parking meter specifications and locations shall be determined by the City of Hackensack;

Utility Accessories:

1. Utility boxes, meters, manhole covers and fire hydrants should be coordinated with other streetscape accessories;
2. Utilities should be readily accessible and placed so as not to obstruct visibility and movement;
3. Utility locations should minimize visual and physical impact as much as possible and should blend in with the surroundings or enhance the area;

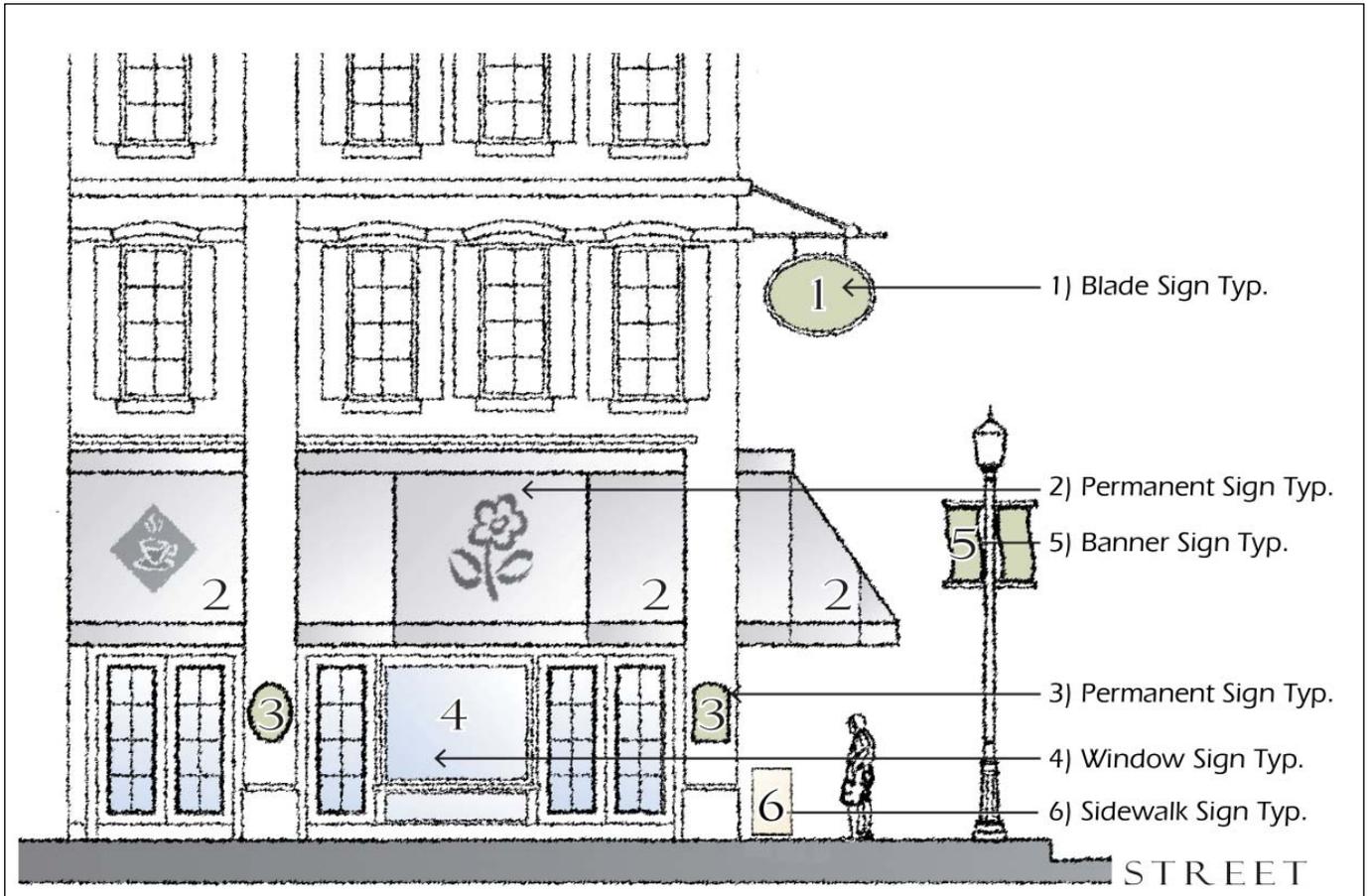
Traffic Calming:

1. The width of curb radii should be minimized. Wide sweeping intersections should be discouraged;
2. Crosswalks should be provided at all intersections and constructed with different paving materials than the street;
3. For internal roads, speed bumps, speed tables and bump-outs should be considered;
4. For River Street a maximum of four roadway connections are permitted. The main intersection shall remain at River and Atlantic Street with the other three connections being right-in / right-out turning movements;
5. Pedestrian improvements to River Street should be considered during the site plan approval phase which may include a landscaped median in the middle of River Street to facilitate safer pedestrian access from the downtown to the Hackensack River Walkway.

SIGNAGE STANDARDS:

The recommendation of this Redevelopment Plan is to establish a committee to review the signage for this project during site plan review by the Planning Board.

1. A signage package for the entire project should be submitted for review and overall signage approval.
2. Individual phases should submit signage for review by the Planning Board and should be consistent with the overall signage package approval.
3. It is recommended pylon signs not be permitted as a part of the overall signage package.



Signage locations typ.



Conceptual Development Plan - Option 'A'
with Retail on Lower Level



Conceptual Development Plan - Option 'B'
with Retail on Lower Level



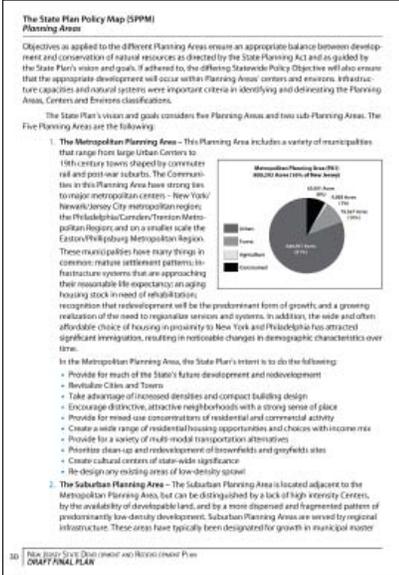
Conceptual Development Plan - Option 'C'
with Retail on Lower Level

4.0 RELATIONSHIP OF THE REDEVELOPMENT PLAN TO STATE / COUNTY / LOCAL MASTER PLAN STUDIES:

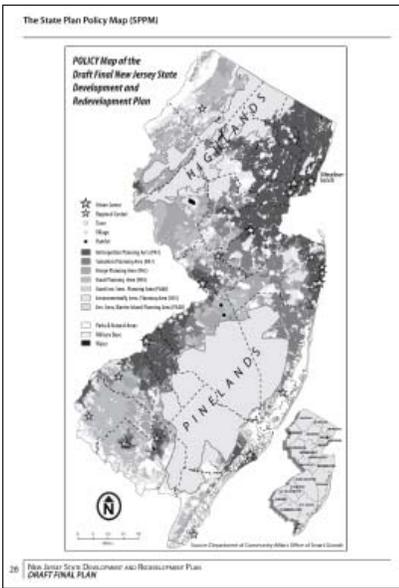
A. State Development and Redevelopment Plan (SDRP):

In reviewing the New Jersey State Development and Redevelopment Plan Volumes 1 - 4 the following information pertains to goals and policies for a program of rehabilitation which discuss the development and redevelopment policies for urban areas.

1. Volume II – State Plan Goals and Policies include the following:
 - a. Revitalize the State’s Cities and Town Centers: Revitalize New Jersey’s cities and towns by investing wisely and sufficiently in improvements in their infrastructure systems, public spending programs, tax incentives and regulatory programs to leverage private investment and to encourage infill and redevelopment in ways that are consistent with the State Plan’s vision and goals.
 - b. Conserve the State’s Natural Resources and Systems: Conserve the State’s natural resources and systems by planning the location and intensity of growth to maintain natural resource and systems capacities and make the necessary infrastructure investments to protect natural resources and systems in ways that guide growth and development in ways that are consistent with the State Plan’s vision and goals.
 - c. Promote Beneficial Economic Growth: Promote beneficial economic growth in locations and in ways that improve the quality of life and the standard of living for all New Jersey residents by providing infrastructure in advance of, or concurrent with, the impacts of new development sufficient to maintain adequate facility standards, by encouraging partnerships and collaborative planning with the private sector and by capitalizing on the State’s strategic location, economic strengths including its existing business enterprises, entrepreneurship, the research and development capacity of its institutions of higher learning, skilled workforce, cultural diversity and logistic facilities in ways that are consistent with the State Plan’s vision and goals.
 - d. Protect the Environment, Prevent and Clean up Pollution: Protect the environment, prevent and clean up pollution by planning for growth in compact forms at locations, densities and intensities that protect land, air and water quality, allow expeditious regulatory reviews and encourage multi-modal transportation alternatives to the automobile to help achieve and maintain acceptable air quality standards.....
 - e. Provide Adequate Public Facilities and Services at Reasonable Cost: Provide adequate public facilities and services by supporting investments based on comprehensive planning and by providing financial incentives for jurisdictions that cooperate in providing public infrastructure and shared services. Encourage the use of infrastructure needs assessments and life-cycle costing. Provide adequate public facilities in ways that are consistent with the State Plan’s vision and goals.
 - f. Provide Adequate Housing at Reasonable Cost: Provide adequate housing at reasonable cost through public/private partnerships that create and maintain a full range of attractive, affordable, and environmentally sensitively-designed and developed housing, particularly for those New Jersey State Development and Redevelopment Plan most in need, at densities and locations that provide greater efficiencies and serve to support public transportation alternatives and reduce commuter time and expense and easily accessible to employment, retail, cultural, civic and recreational opportunities to reduce housing and commuting costs in ways that are consistent with the State Plan’s vision and goals.



New Jersey SDRP - The State Plan Policy Map "Metropolitan Area 1"

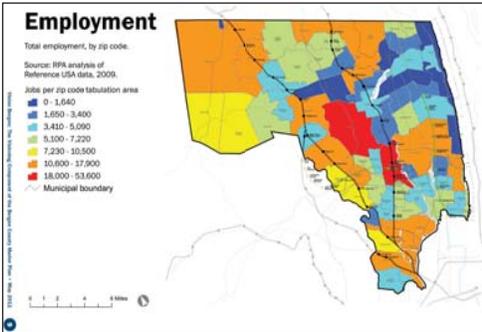


New Jersey State Development and Redevelopment Plan Policy Map

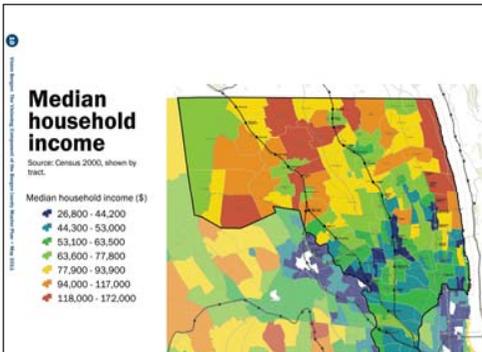
- g. Preserve and Enhance Areas with Historic, Cultural, Scenic Open Space, and Recreational Value: Preserve, enhance, and use historic, cultural, scenic and recreational assets by collaborative planning, design, investment and management techniques. Locate and design development and redevelopment and supporting infrastructure to improve access to and protect these sites. Support the important role of the arts in contributing to community life, civic beauty and redevelopment in ways that are consistent with the State Plan's vision and goals.
 - h. Ensure Sound, Coordinated and Integrated Statewide Planning: Ensure sound, coordinated and integrated statewide planning by using the State Plan as a guide to planning and growth related decisions at all levels of government in ways that are consistent with the State Plan's vision and goals.
 - i. Increase Energy Efficiencies and Reduce Greenhouse Gas Emissions: Increase Energy Efficiencies and Reduce Greenhouse Gas Emissions by promoting the improved coordination and integration of transportation planning and land-use planning and decision-making to reduce vehicle miles traveled (VMT); and by the citing, development, design and use of green-building construction materials and techniques in ways that are consistent with the State Plan's vision and goals.
2. The State Planning Act (N.J.S.A. 52:18A-196 et. seq.) states:
- a. "Among the goals of the act is the following: ...conserve its natural resources, revitalize its urban centers, protect the quality of its environment, and provide needed housing and adequate public services at a reasonable cost while promoting beneficial economic growth, development and renewal."
 - b. "It is in the public interest to encourage development, redevelopment and economic growth in locations that are well situated with respect to present or anticipated public services and facilities, giving appropriate priority to the redevelopment, repair, rehabilitation or replacement of existing facilities and to discourage development where it may impair or destroy natural resources or environmental qualities that are vital to the health and well-being of the present and future citizens of this state." (N.J.S.A. 52:18A-196 (d))
3. The State Plan Policy Map (SPPM)
- a. Recognizes that New Jersey requires different approaches in its Metropolitan, Suburban, Rural and Environmentally Sensitive Planning Areas. The New Jersey State Development and

Redevelopment Plan adopted by the State Planning Commission, places the City of Hackensack in Planning Area 1 - the Metropolitan Planning Area and states:

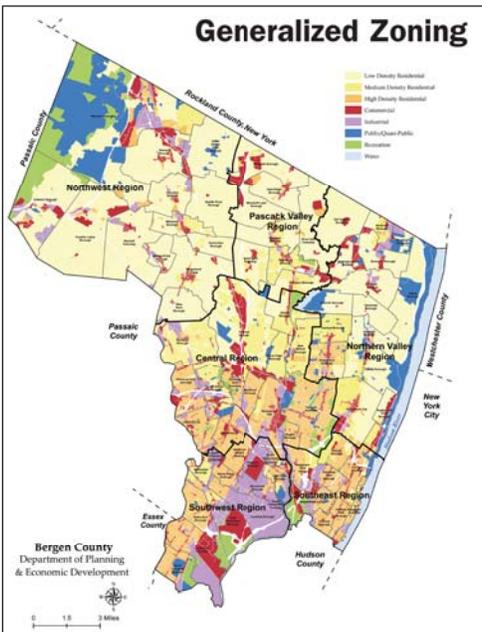
- b. The Metropolitan Planning Area: This Planning Area includes a variety of municipalities that range from large Urban Centers to 19th century towns shaped by commuter rail and post-war suburbs. The Communities in this Planning Area have strong ties to major metropolitan centers – New York/Newark/Jersey City metropolitan region; the Philadelphia/Camden/Trenton Metropolitan Region; and on a smaller scale the Easton/Phillipsburg Metropolitan Region.
- c. These municipalities have many things in common: mature settlement patterns; infrastructure systems that are approaching their reasonable life expectancy; an aging housing stock in need of rehabilitation; recognition that redevelopment will be the predominant form of growth; and a growing realization of the need to regionalize services and systems. In addition, the wide and often affordable choice of housing in proximity to New York and Philadelphia has attracted significant immigration, resulting in noticeable changes in demographic characteristics overtime.
- d. In the Metropolitan Planning Area, the State Plan’s intent is to do the following:
 - Provide for much of the State’s future development and redevelopment;
 - Revitalize Cities and Towns;
 - Take advantage of increased densities and compact building design;
 - Encourage distinctive, attractive neighborhoods with a strong sense of place;
 - Provide for mixed-use concentrations of residential and commercial activity;
 - Create a wide range of residential housing opportunities and choices with income mix;
 - Provide for a variety of multi-modal transportation alternatives;
 - Prioritize clean-up and redevelopment of brownfields and greyfields sites;
 - Create cultural centers of state-wide significance;
 - Re-design any existing areas of low-density sprawl;



Draft Bergen County Master Plan
Employment Data



Draft Bergen County Master Plan
Medium Household Income



Draft Bergen County Master Plan
General Zoning

B. Bergen County Master Plan:

In reviewing the Final Draft Bergen County Master Plan the following information pertains to goals and policies for a program of rehabilitation which discuss the development and redevelopment policies for urban areas.

1. Bergen County Master Plan Overview:

- a. The Bergen County Department of Planning and Economic Development is undertaking a county-wide planning effort that will result in the first County Master Plan in some time.
- b. The Master Plan will create a unifying vision for the County’s 70 municipalities and help them plan for sustainable growth while protecting environmental resources. Development of the Master Plan will be a collaborative process involving the County, municipalities, regional agencies, public and private sector stakeholders and Bergen County citizens.

2. Draft Report:

- a. The Draft Report provided on the Bergen County Master Plan website includes the following descriptions:
 - Future growth will primarily occur through redevelopment and infill;
 - Redevelopment provides opportunities to create new public spaces and green areas in places that have none;
 - Redevelopment on a large scale using green guidelines has the potential over time to significantly ameliorate many of the county’s storm water run-off and water quality issues;
 - Redevelopment can occur spontaneously and be privately driven, or it can take place as a result of a public initiative, usually under the jurisdiction of a local redevelopment agency.



Spheres of Influence Map
2001 Master Plan - Kasler Associates, PA

C. City of Hackensack 2001 Master Plan Study and 2009 Reexamination Report:

The City's Master Plan report provided strategies for redevelopment in the Study Area which included developing the downtown based on spheres of influence. The Master Plan report noted that:

1. Goals and Objectives stated in the Master Plan Reports excerpts include:
 - a. Maintain and enhance the quality of established neighborhoods in Hackensack and promote compatibility of new development with existing or specifically defined character;
 - b. Encourage public and private redevelopment to assist in the rehabilitation of areas in need of improvement and upgrading including utilization of State and Federal Assistance programs, where applicable, as well as public / private partnerships;
 - c. Provide housing opportunities and a variety of housing for various income levels or the population, including low and moderate income housing, middle income housing and senior citizen housing; encourage multi-family and mixed-use development and redevelopment with in the central business district
 - d. Improve the quantity, quality and availability of parks and open space including active and passive recreational facilities, neighborhood parks and environmentally sensitive areas. Encourage open space within major new developments and redevelopment. Promote the establishment of a publicly accessible linear greenway (riverside) park along the Hackensack River;
 - e. Encourage adaptive re-use of historic and character defining structures, where appropriate; encourage context sensitive design of replacement structures;
 - f. Promote adequate community services for all portions of Hackensack with an emphasis on improving the quality and adequacy of education, stormwater drainage, sewer, transportation, parking and recreation facilities;
 - g. Promote and upgrade the downtown area of Hackensack including the four spheres of influence with an emphasis on assuring a vibrant, mixed use and appealing downtown;

2. Strategies for Redevelopment:

In the City's Master Plan the report provides strategies for redevelopment in the Study Area which include developing the downtown based on spheres of influence. These include:

- a. Main Street is too long to be developed continuously, and thus must be developed in "spheres of influence"; the government sector, the banking / educational / cultural sphere containing the urban node, the retail sector which would focus on conventional and household shopping and the Packard area
 - b. The downtown study should focus on the following:
 - The potential for redevelopment of the Main Street Area;
 - A reduction in the scale of the retail/shopping sector. Main Street is too long to be developed continuously. The downtown area is not pedestrian friendly;
 - The provision of additional parking and the dichotomy between long term (monthly) parking and short term shopper parking needs to be resolved;
 - The ERA report believes that (mid-rise) apartment uses which can overlook the Hackensack River in the downtown are potentially marketable and can provide an alternative market for goods and services in the downtown corridor.
3. The City's 2009 Master Plan Re-examination Report contains a significant amount of narrative related to areas in need of rehabilitation which included the following:
- a. Regarding Areas in Need of Rehabilitation the document states: "A viable alternative to the use of eminent domain is available to the City for stimulating private redevelopment in the form of Areas in Need of Rehabilitation. This simpler approach to side-scale rehabilitation and redevelopment does not involve eminent domain, but it provides a means for making substantial progress at minimal cost.
 - b. Regarding the program of Rehabilitation the document states: "In addition, the Mayor and Council must determine that a program of rehabilitation, as defined in the LRHL, may be expected to prevent further deterioration and promote the overall development of the community.
 - c. Regarding Infrastructure Age the document states: "While the City's housing stock as of the 2000 US Census indicated that 45% was built before 1960, it is probable that more discreet, yet fairly extensive areas of the City would qualify under the age of housing criteria. The age of water and sewer infrastructure may also qualify designation of the entire city as an area in need of rehabilitation.
 - d. Regarding Rehabilitation Area Options the document states: "The City's options for redevelopment in an area in need of rehabilitation are basically the same as for an area in need of redevelopment, except that eminent domain cannot be used unless a formal designation of one or more properties as an area in need of redevelopment is made by the Mayor and Council. Thus, proceeding with the rehabilitation route does not preclude pursuing special properties for designation as an area in need of redevelopment if needed later on. In addition, the rehabilitation designation requires a simple resolution (no public hearing or special notice required) by the Mayor and Council, thus avoiding the time and expense of a redevelopment designation study.
 - e. The document also states: "The available options for an area designated as an area in need of rehabilitation include the following:

- Planning for redevelopment in a collaborative process with property owners, rather than an adversarial one, resulting in a shorter path to actual re-investment in the designated area.
 - Property owners can be afforded tax abatement over five years to allow their capital to be directed to property upgrades and expansions, which ultimately enhance the tax base due to the positive effects on the subject property and those around it.
 - The City may prepare a redevelopment plan for any area designated for rehabilitation, select a redeveloper, provide special redevelopment zoning standards and design criteria and phase the development over a predetermined schedule.
 - Off-site improvements can be accomplished without the need for meeting the “rational nexus” criteria.
 - Each project can be guided by a redeveloper’s agreement or simply the requirements of the redevelopment plan.
4. Regarding the Rehabilitation Approach the document states: “The area in Need of Rehabilitation approach has been effectively used in number of New Jersey Municipalities and holds great promise for the City of Hackensack. Therefore this report strongly recommends the pursuit of this approach in all areas of the City that meet the criteria.
5. The City’s 2001 and 2009 Master Plan Reports provide strategies for redevelopment which includes the properties under investigation. The 2001 and 2009 Master Plan report states:
- a. Master Plan Goals and Objectives
 - Maintain and enhance the quality of established neighborhoods in Hackensack and promote compatibility of new development with existing or specifically defined character;
 - Encourage public and private redevelopment to assist in the rehabilitation of areas in need of improvement and upgrading including utilization of State and Federal Assistance programs, where applicable, as well as public / private partnerships;
 - Provide housing opportunities and a variety of housing for various income levels or the population, including low and moderate income housing, middle income housing and senior citizen housing; encourage multi-family and mixed-use development and redevelopment with in the central business district
 - Improve the quantity, quality and availability of parks and open space including active and passive recreational facilities, neighborhood parks and environmentally sensitive areas. Encourage open space within major new developments and redevelopment. Promote the establishment of a publicly accessible linear greenway (riverside) park along the Hackensack River;
 - Encourage adaptive re-use of historic and character defining structures, where appropriate; encourage context sensitive design of replacement structures;
 - Promote and upgrade the downtown area of Hackensack including the four spheres of influence with an emphasis on assuring a vibrant, mixed use and appealing downtown;

6. City of Hackensack: 2009 Master Plan Re-examination Report:
 - a. Regarding Infrastructure Age the document states: “While the City’s housing stock as of the 2000 US Census indicated that 45% was built before 1960, it is probable that more discreet, yet fairly extensive areas of the City would qualify under the age of housing criteria. The age of water and sewer infrastructure may also qualify designation of the entire city as an area in need of rehabilitation.”
 - b. Regarding Rehabilitation Area Options the document states: The City’s options for redevelopment in an area in need of rehabilitation are basically the same as for an area in need of redevelopment, except that eminent domain cannot be used unless a formal designation of one or more properties as an area in need of redevelopment is made by the Mayor and Council. Thus, proceeding with the rehabilitation route does not preclude pursuing special properties for designation as an area in need of redevelopment if needed later on. In addition, the rehabilitation designation requires a simple resolution (no public hearing or special notice required) by the Mayor and Council, thus avoiding the time and expense of a redevelopment designation study.
 - c. Planning for redevelopment in a collaborative process with property owners, rather than an adversarial one, resulting in a shorter path to actual re-investment in the designated area.
 - d. Property owners can be afforded tax abatement over five years to allow their capital to be directed to property upgrades and expansions, which ultimately enhance the tax base due to the positive effects on the subject property and those around it.
 - e. The City may prepare a redevelopment plan for any area designated for rehabilitation, select a redeveloper, provide special redevelopment zoning standards and design criteria and phase the development over a predetermined schedule.
 - f. Off-site improvements can be accomplished without the need for meeting the “rational nexus” criteria.
 - g. Each project can be guided by a redeveloper’s agreement or simply the requirements of the redevelopment plan.

D. Neighboring Community Master Plans:

Information for the Master Plans of the adjacent municipalities have been included as stated in the 2009 Master Plan Reexamination Report which includes the following information:

1. Borough of Bogota: The 2003 Master Plan for Bogota recommends a rezoning of properties located in the Planned Development zone. This zone directly abuts the City's southeast boundary. The planned development zone has existed since the last master plan was adopted and no planned development has occurred on these parcels for the past ten years. Currently, these properties contain a driving range and have been there for a long time. The Borough's master plan therefore recommends that this area be zoned for 1-2 zones, which permits light-industry use, warehouses, and offices, indoor and outdoor recreational use.
2. Borough of Hasbrouck Heights: The 2003 Master Plan Reexamination Report recommends that the Borough should consider redeveloping portions of lands along Route 17, which connects Hasbrouck Heights to Hackensack City. However, these projects, if and when they are built, will not significantly impact Hackensack.
3. Borough of Little Ferry: The Borough's 2004 Master Plan Reexamination Report recommends significant redevelopment along the waterfront. The plan recommends developing the area around Bergen Turnpike and Valley Road intersection that currently contains an under utilized shopping center to be redeveloped to include hotel, high-end large retail anchors, restaurants and a mix of offices. The plan recommends redevelopment along the waterfront that would include low-rise (1-3 story) mixed-use development with an array of recreational amenities and pedestrian walkways along the riverfront. The recommended projects are consistent with the development currently occurring along River Street in Hackensack.
4. Borough of Maywood: The Borough of Maywood 2003 Master Plan recommends that redevelopment study should be undertaken for areas along the southwesterly side of the Borough that have access from Route 17. This, however, will not impact Hackensack.
5. Borough of Paramus: The 2005 Master Plan Reexamination Report recommends that the Borough should evaluate for any areas in need of redevelopment within the Borough. The Plan also suggests rezoning certain residential properties that directly abut the Maywood Borough boundary. However, this will not impact Hackensack.
6. Borough of River Edge: The Borough does not have any significant plans that would affect Hackensack.
7. Borough of South Hackensack: The Borough prepared a land use element of the Master Plan in 2001; however, no changes are proposed at this time.
8. Borough of Teterboro: The 2006 Master Plan Reexamination Report recommends that the Borough should evaluate potential properties within the existing industrial area that are in need of redevelopment.
9. Borough of Teaneck: The 2006 Master Plan Reexamination Report recommends that the Borough should evaluate potential properties within the existing industrial area that are in need of redevelopment.
10. Borough of Lodi: The Borough does not have any significant plans that would affect Hackensack.
11. Village of Ridgefield Park: Portions of the industrial area that abut the Hackensack boundary to the east are being redeveloped. As part of the riverside park system plan, a new bike path is proposed along the river. These improvements are consistent with the waterfront redevelopment projects that Hackensack and Bergen County are proposing."

Temporary and Permanent Relocation:

The Local Redevelopment and Housing Law requires that any redevelopment plan make adequate provision for temporary or permanent relocation of any resident in the project area. As there are no residential buildings in the redevelopment area, there is no need to make provision for temporary or permanent relocation of any resident.

Identification of Property to be Acquired:

The Local Redevelopment and Housing Law requires that any redevelopment plan identify any property within the redevelopment area which is proposed to be acquired in accordance with the redevelopment plan.

While it is the City's fervent hope that a private developer will obtain the necessary property interests to privately develop the entire redevelopment area; the City does not rule out acquiring each and every property in the redevelopment area in order to redevelop the area in accordance with the goals and objectives stated in this redevelopment plan.

Affordable Housing:

As of the date of adoption of the resolution finding the area to be in need of redevelopment, no affordable housing units are to be removed from the City's inventory as a result of the implementation of this redevelopment plan. The developer shall contribute to the rehabilitation fund to assist the City in meeting a portion of its rehabilitation obligation and the fee would be determined on a pro-rata basis.

Traffic Impact Analysis:

A traffic impact analysis should be provided as a part of the phase one site plan submission to the Planning Board. The analysis should forecast additional traffic associated with the entire development, determine vehicular and pedestrian improvements necessary to accommodate the proposed development to help ensure safe and reasonable traffic conditions and reduce the negative impacts created by the development.

Long Term Financing Considerations:

Due to the size, scale, vital location and anticipated catalyst nature of the potential projects contemplated under this plan, the City anticipates that it would be eligible for a long term tax exemption for a period of up to 30 years if the project commences within five (5) years of plan adoption. After that time, the specific economic conditions would have to be reevaluated to determine the duration of an exemption that would be deemed financially feasible and appropriate. The actual entry of any financial agreements for a long term tax exemption are subject to governing body approval under the processes required by law.

Project Signage:

The redeveloper shall erect signage at locations to be determined by the Redeveloper and City within 30 days of receiving approval from the Planning Board that contains a rendering or renderings of the finished Project and other details concerning the Project that shall be agreed to by the Redeveloper and City Manager.